

City of Everett

Central Waterfront Redevelopment Plan



A Subarea Plan of the Everett Growth Management Comprehensive Plan

Effective January 25, 2013 (Ordinance No. 3311-13)

Acknowledgements

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SECTION 1. CENTRAL WATERFRONT REDEVELOPMENT PLAN

A. Introduction – Purpose – Background Information

The Kimberly-Clark Worldwide, Inc. (Kimberly-Clark or K-C) closed its pulp and paper mill on the downtown Everett waterfront in April, 2012 after unsuccessful attempts to find a buyer that would maintain the operation and local workforce. While opening up a fifty-six acre waterfront site for alternative use, the closure also eliminated over 700 high wage jobs in the local economy. Prior to the closure of the mill, the City of Everett had never addressed the question about how the site should be reused or redeveloped in the event of closure. The City had strongly supported the continued presence of the Kimberly-Clark mill as a good employer and corporate citizen in the community. When faced with the prospect of mill closure, the City decided to initiate a re-use planning process to determine the long term best interests of the community concerning future uses on the Kimberly-Clark site and in the immediate vicinity.

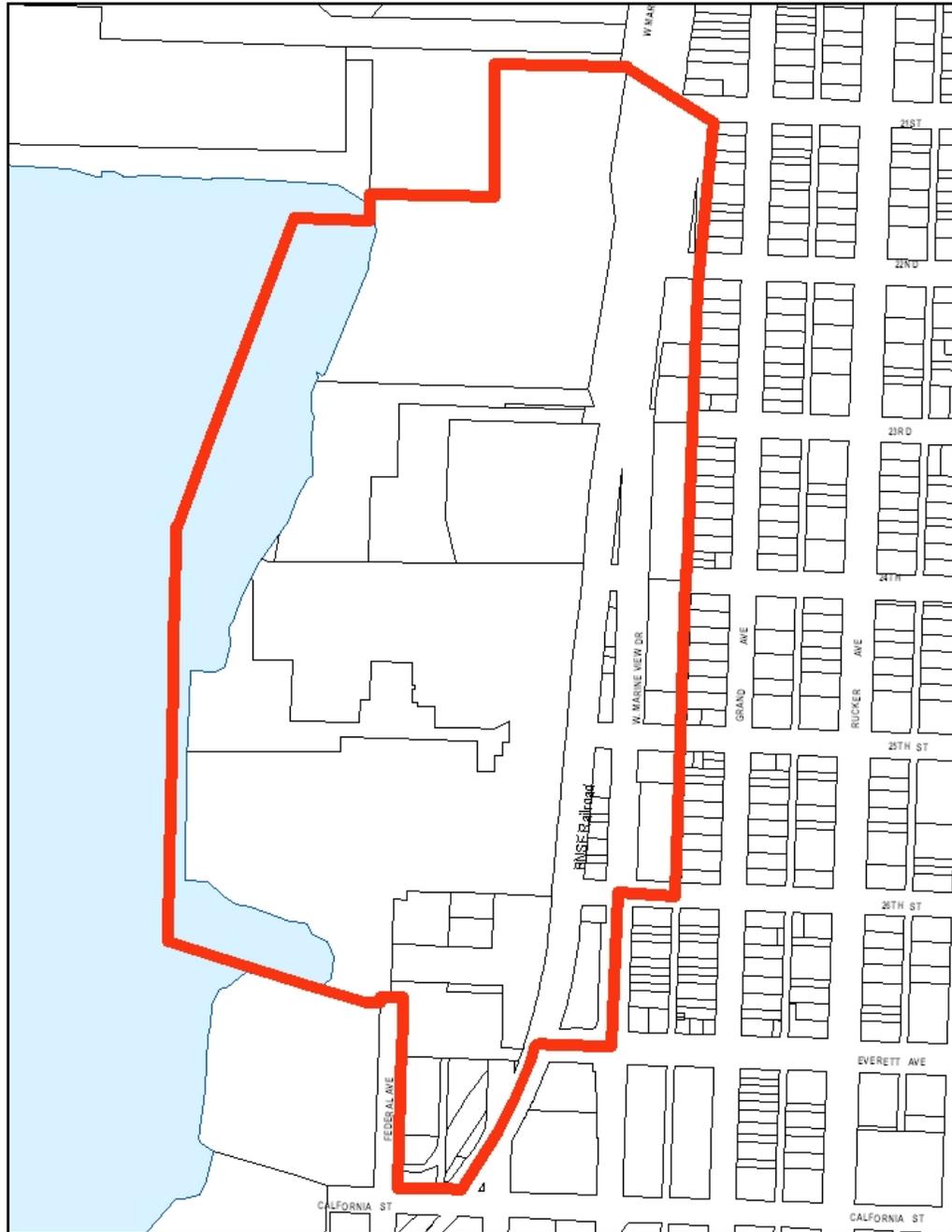
Moratorium Ordinance

The City adopted Ordinance #3260-12 (see Appendix 1) establishing a moratorium on development of the site while a preferred reuse plan is prepared. The affected area is indicated in Figure A. The City is keenly interested in what alternative uses may be developed on the site. In particular, the City is interested in uses that provide many high wage jobs, offer opportunities for public access to the waterfront via portions of the site or elsewhere, and provide benefits to downtown and the surrounding neighborhoods. The Ordinance includes a number of elements the City is to consider in the re-use planning process, including the following:

- Public participation
- Property owner input
- Maximizing economic development potential of the property
- Range of permitted uses appropriate for site
- Opportunities to integrate public access, environmental enhancement, and other public benefits
- Private property rights
- Compatibility with Port and Navy operations
- How potential future uses may affect surrounding uses, downtown and the community
- Environmental cleanup requirements
- Clean and sustainable uses

Central Waterfront Redevelopment Plan

Figure A
Central Waterfront Planning Area



B. Properties in the Central Waterfront Planning Area

Property owners in the Central Waterfront Planning Area include Kimberly-Clark, BNSF, the City of Everett, the Snohomish PUD No. 2, and four private owners. See Figure B. The vast majority of the area is Kimberly-Clark owned property zoned M-2 located west of West Marine View Drive. Kimberly-Clark also owns parcels east of West Marine View Drive that are currently used for parking, but are zoned multiple family. The multiple family zoned properties are subject to either the Rucker-Grand Historic Overlay Zone standards or the Core Area Residential Design Guidelines. See Figure C for zoning designations.

Two privately owned parcels located south of the Kimberly-Clark mill site (Ronan and Mobil Oil Corp.) are zoned M-2 and are large enough to accommodate industrial uses; however, uses cannot be water-dependent since the parcels are separated from the water's edge by property owned by the Port of Everett and by the Federal Avenue right-of-way.

A privately owned parcel located between West Marine View Drive and the BNSF right-of-way has historically been used for parking for the abutting Milltown Credit Union building, which is located on land owned by Kimberly-Clark. This parcel is probably too small to support a viable industrial use under the current M-2 zoning.

A privately owned parcel of land located on the southeast corner of West Marine View Drive and 25th Street is zoned B-1 (Neighborhood Business) and has had a history of use as a convenience store, boat repair business, and other commercial uses.

C. Public Participation

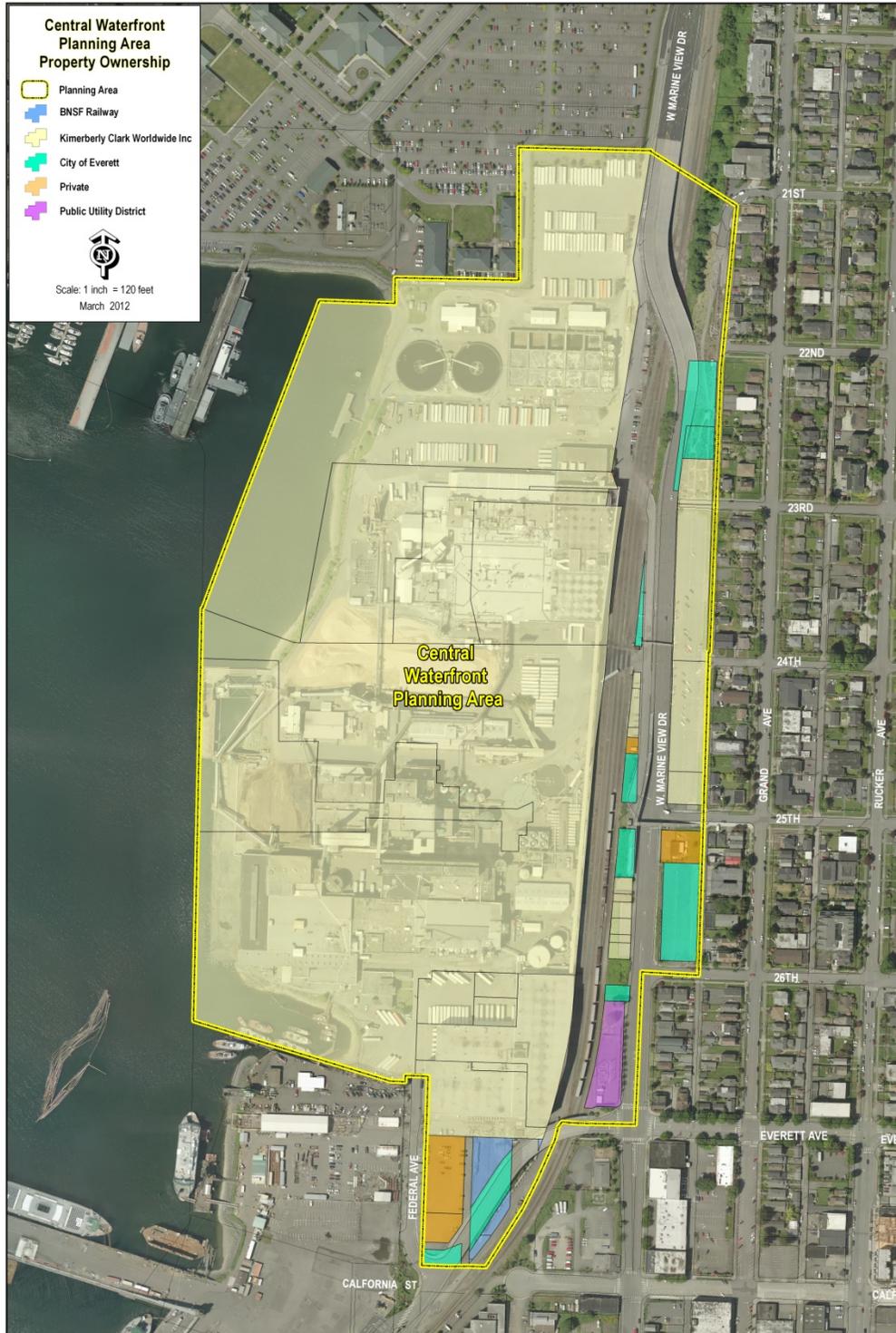
Public meetings, Planning Commission workshops and hearings

The City hosted a community public meeting on April 3, 2012 to provide information about the planning process, existing policies and regulations, and to get feedback from the public about priorities for the planning process. Over 100 citizens attended this meeting, which included audience responses to several questions about community priorities, and an interactive “dot exercise” in which people were allowed to identify their individual priorities for future uses, development standards, or community benefits.

The Planning Commission also held public workshops on June 19, 2012, July 17, 2012, and August 21, 2012 to consider presentations from the City's consultant team and staff concerning economics, public access, cleanup requirements, and future land uses. A public hearing was held on September 18, 2012 on the four alternative land use scenarios and a public hearing was held

Central Waterfront Redevelopment Plan

Figure B



Central Waterfront Redevelopment Plan

Figure C



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on the Preferred Alternative on October 23, 2012. The public was able to offer their comments, ask questions, and express concerns at each public workshop and public hearing. Kimberly-Clark's commercial real estate broker provided information about the nature of responses to marketing efforts, the type of uses indicating an interest in the property, and the company's preference to maintain the existing M-2 zoning and to provide public access off-site since water-dependent industries were not generally compatible with public recreational uses.

While most of the public comments focused on potential uses and redevelopment of the Kimberly-Clark mill property, several comments were offered concerning the disposition of property within the Bayside neighborhood community garden, a portion of which is located on land owned by Kimberly-Clark. A few comments were offered about the potential redevelopment of the parking lots located on the east side of West Marine View Drive, which also are owned by Kimberly-Clark and zoned for multiple family dwellings.

See Planning Commission minutes in Appendix 2.

Questionnaire

The City also issued a public questionnaire in April, 2012, to gather public feedback concerning the reuse plan. Approximately 200 surveys were returned addressing the following 4 questions:

1. What should the city's priorities be in planning for the re-use and redevelopment of the central waterfront area?

Top responses included:

- replacing lost jobs
- creating a physical public access connection to the water's edge
- environmental cleanup of the site

2. In evaluating the economic impact of the future re-use and redevelopment of the central waterfront area, which factors should be given the highest priority?

Top responses included:

- potential to generate business activity downtown and in the community
- potential to stimulate development in downtown and surrounding neighborhoods
- potential to generate the greatest number of jobs

3. What are the most important development standards that should be placed on uses that may be established in the future in central waterfront area?

Top responses included:

- public access to water's edge or view opportunity
- shoreline edge habitat restoration
- building design standards and guidelines

4. What type of uses should the re-use and redevelopment plan allow?

Top responses included:

- uses that can coexist with public access
- uses that provide a large number of family wage jobs
- commercial uses, such as retail, restaurant, offices, entertainment

See Appendix 3 for a detailed summary of responses to each question. See Appendix 4 for a summary of written comments provided in conjunction with the questionnaire.

D. Compatibility with Port of Everett and Naval Station Everett Operations

Ordinance #3260-12 directed staff to consider in the reuse planning process the compatibility of future uses with the operations of the Port of Everett maritime terminals and Naval Station Everett. Each abuts the Kimberly-Clark mill site, separated from the mill property by a chain link fence topped with barbed wire. Both facilities are secure facilities that are not open to the general public. In discussions with both organizations, the overriding concern of each is maintaining the safety and security of their facilities as the Kimberly-Clark mill site is redeveloped. Each has expressed concern about public access to the inner harbor and the ability of people to use the harbor for recreational boating or kayaking. Each has also expressed concerns about increased opportunities for terrorist threats that could arise from increased access by the general public. Each has indicated there will be a need for increased vigilance on their part to monitor activities on the former mill site to protect the ongoing security of their facilities and operations.

The Port of Everett has indicated they are considering the acquisition of the Kimberly-Clark mill property. If the Port were to purchase the property, it would be to expand the capacity of their maritime terminals. Naval Station Everett has indicated that they are not interested in purchasing the property.

Naval Station Everett submitted a formal comment letter dated August 14, 2012, stating concerns about the potential impact of new uses on the site in proximity to the Naval Station (see correspondence in Appendix 7). They requested 80 foot setbacks from their existing buildings and parking areas, which would result in 30 – 46 foot setbacks from the shared Navy / Kimberly-Clark property line to be consistent with current Department of Defense Anti-Terrorism Force Protection policies. The Navy also requested that any proposed use be required to provide a “comprehensive industrial site security and public safety plan as a condition of site approval,” with an opportunity for the Navy to provide input to the plan prior to City approval. The Navy also expressed concerns about compatibility of specific uses and forms of public access, summarized below:

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- A facility that manufactures or stores explosive or flammables would be an incompatible use
- A facility that supports aircraft presence, such as a helipad, would be an incompatible use
- Windows on buildings facing north toward Naval Station Everett should be constructed using opaque glass to preclude covert activities directed at the Navy
- Public access to the former Kimberly-Clark mill site should not be allowed due to security concerns. At a subsequent discussion, the Navy indicated that public access would be acceptable provided recreational users are kept out of the harbor, and that public access areas maintain a minimum separation from facilities located on base.
- Future businesses that use electromagnetic frequencies in their operations should submit an “EMR Frequency Spectrum Plan” for review by the Navy, with approval contingent on compatibility with Navy operations
- Any use that requires additional marine traffic should submit a “Port Operations Plan” for review by the Navy
- Any plans for proposed piers or wharfs should be submitted for review by the Navy to ensure no impact to Navy operations
- Recreational boating uses in the harbor would be incompatible with Navy operations

The Navy also requested that standards be developed to address noise, glare, and late night operations to prevent negative impact to Navy facilities.

E. Building Demolition and Site Cleanup

Kimberly-Clark has started to demolish buildings on the mill site and proposes to clean up any contaminated soils as quickly as possible in order to prepare the site for re-use and redevelopment. The company wants to leave a clean site that will allow a future user or users to continue to provide business and employment opportunities on this waterfront property. The moratorium on development during the planning process does not apply to demolition permits or remediation activities. Most buildings will be removed from the site by early 2013.

Kimberly-Clark has worked closely with the Washington Department of Ecology and developed an Agreed Order for conducting a remedial investigation, feasibility study and preparing a cleanup action plan for the upland portion of the site. There are also contaminated sediments in the East Waterway in the harbor abutting the site, including on submerged lands owned by Kimberly-Clark. The in-water cleanup will take place on a separate track from the uplands, as there are several responsible parties and in-water cleanup is more complicated.

Kimberly-Clark is in the process of demolishing and removing most buildings and above grade structures from the site. Because Ecology recognizes the importance of timely redevelopment and use of the property, it has indicated that it strongly supports an approach that enables timely

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remediation and redevelopment of this prime waterfront property to proceed in a coordinated manner without delaying either essential purpose. Ecology has also indicated that it expects to reach an understanding among the agency, Kimberly-Clark and the successor owner(s) so that redevelopment and productive use of the site resumes while cleanup activities -- which can take several years -- proceeds on the basis of the Agreed Order. K-C anticipates that significant site disruption will occur only during relatively brief periods during the remediation process. This re-use plan assumes that the entire upland portion of the mill site will be usable for any permitted use allowed by this plan. This re-use plan does not attempt to project the time it may take to clean up the site or when redevelopment of the property is likely to begin.

F. On-site Utilities

The Port Gardner deepwater outfall pipeline (DWO) plus major stormwater, combined sewer overflow outfall lines are located on and adjacent to the Kimberly-Clark mill site. The City and Kimberly-Clark are in discussions to determine the ultimate disposition of these facilities.

A 48 inch water transmission main served the paper mill, but since the mill has closed, this water supply has been shut off to the property. A future user of the site could take advantage of this water supply for a water-intensive use. In addition, the presence of the DWO may enable a future site user with significant effluent discharge requirements to access this valuable infrastructure.

G. Description of Alternatives

Based on public feedback, input from the economic analysis of viable uses, comments from the property owner's representative, and an analysis of existing comprehensive plan and shoreline master program policies, the City evaluated 4 land use alternatives. Each of the four land use alternatives assumed industrial or related uses. While there have been some public suggestions for non-industrial uses, the public process has led the City to conclude that the best long term interests of the community are served by continuing to use the M-2 zoned area primarily for industry and employment, with water-oriented commercial uses and public access encouraged, based on the following considerations:

- nature of the public infrastructure that has been constructed to support industrial use
- character of other water-dependent industrial and military uses in the immediate vicinity
- economic analysis of viable uses provided by the City's consultant
- City's downtown redevelopment strategy to promote a mix of commercial and residential uses in the central business district
- community's interest in providing a physical connection to the water's edge
- community's interest in encouraging family wage jobs in Everett

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- potential to incorporate public access, either on-site or in other off-site locations to improve the community's access to the shoreline.
- the comprehensive plan industrial land preservation policy

The four land use alternatives considered were:

1. Existing Regulations – No Action Alternative. Land Use Vision for Existing M-2 zoned areas – No change to existing regulations. All existing M-2 zone uses are permitted. Permitted uses in the shoreline area included water-dependent and water-related industrial uses, water-oriented commercial uses, recreational uses, boating facilities, transportation facilities, and utilities and utility facilities. Outside of shoreline areas (200 feet from Ordinary High Water Mark), a wide variety of uses are permitted. No changes to existing development standards or review processes. No additional restrictions on uses or development standards.

Public Access Concept - Not required on-site unless a non-water-dependent use is established in shoreline jurisdiction. Off-site public access to the shoreline may be substituted in lieu of on-site public access.

Land Use Vision for Properties located east of West Marine View Drive – No change to existing zoning. The existing R-3H, R-5 and B-1 zone regulations would not change.

2. Water-Dependent and Heavy Industrial. Land Use Vision for Existing M-2 zoned areas – A working waterfront job center including cargo handling, water-dependent manufacturing supported by railroad access, marine services, marine commerce and construction, and Naval Station Everett operations, all supporting a strong regional economy. Would include revisions to permitted M-2 uses, requiring water-dependent / related industrial uses and prohibiting or limiting non-industrial uses to those that serve industrial uses in the area, and water-oriented uses. Would add development standards to prohibit or mitigate off-site impacts related to noise, odor, aesthetics, air quality, and other impacts.

Public Access Concept - Not required on-site unless a non-water-dependent use is established in shoreline jurisdiction. Off-site public access to the shoreline may be substituted in lieu of on-site public access.

Land Use Vision for Properties located east of West Marine View Drive – No change to existing zoning. The existing R-3H, R-5 and B-1 zone regulations would not change.

3. Business Park and Public Access. Land Use Vision for Existing M-2 zoned areas – a high-quality waterfront business park / employment center with primarily non-water-dependent uses focused around a public access area and trail system, connected to downtown, where

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people can work, visit, and enjoy visual contact with the water and waterfront activities. M-2 zone permitted uses and development standards would be amended to promote a high-quality mix of light-industrial uses and limited heavy manufacturing uses with additional standards to minimize impacts to surrounding properties.

Public Access Concept – Public access is required on-site for non-water-dependent uses in shoreline jurisdiction. If non-water-dependent uses are established along the shoreline, environmental / habitat restoration will be required where feasible.

Land Use Vision for Properties located east of West Marine View Drive – No change to existing zoning. The existing R-3H, R-5 and B-1 zone regulations would not change.

4. Water-Dependent and Non-Water-Dependent Industrial Mixed Use. Land Use Vision for Existing M-2 zoned areas – an employment center for water-dependent uses along the harbor with a mix of water-dependent uses and non-water-dependent industrial uses outside of shoreline jurisdiction, with quality development and design standards.

Public Access Concept – Public access is encouraged on-site where not in conflict with water-dependent uses, and required on-site for non-water-dependent uses in shoreline jurisdiction.

Land Use Vision for Properties located east of West Marine View Drive – No change to existing zoning. The existing R-3H, R-5 and B-1 zone regulations would not change.

Figures D through G illustrate each of these land use alternatives for those portions of the Central Waterfront Planning Area located west of West Marine View Drive.

H. Shoreline Uses and Public Access Concepts

Appendix 5 includes a description of the State Shoreline Management Act (SMA) requirements for public access to the shoreline and the guidance provided in the Shoreline Master Program (SMP) Handbook for implementing the requirements. Everett's SMP implements the requirements of the SMA and requires that public access be required, as noted immediately above in section G.2. and discussed further below, for non-water-dependent development to the extent allowed by law in the review of all shoreline permits with some exceptions. It requires that public access be provided on site, except for projects that meet one of the following specific criteria, including, but not limited to,

- The project is in the Deep Water Port Environment (Kimberly-Clark mill site is in this environment) or

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Figure D



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Figure E



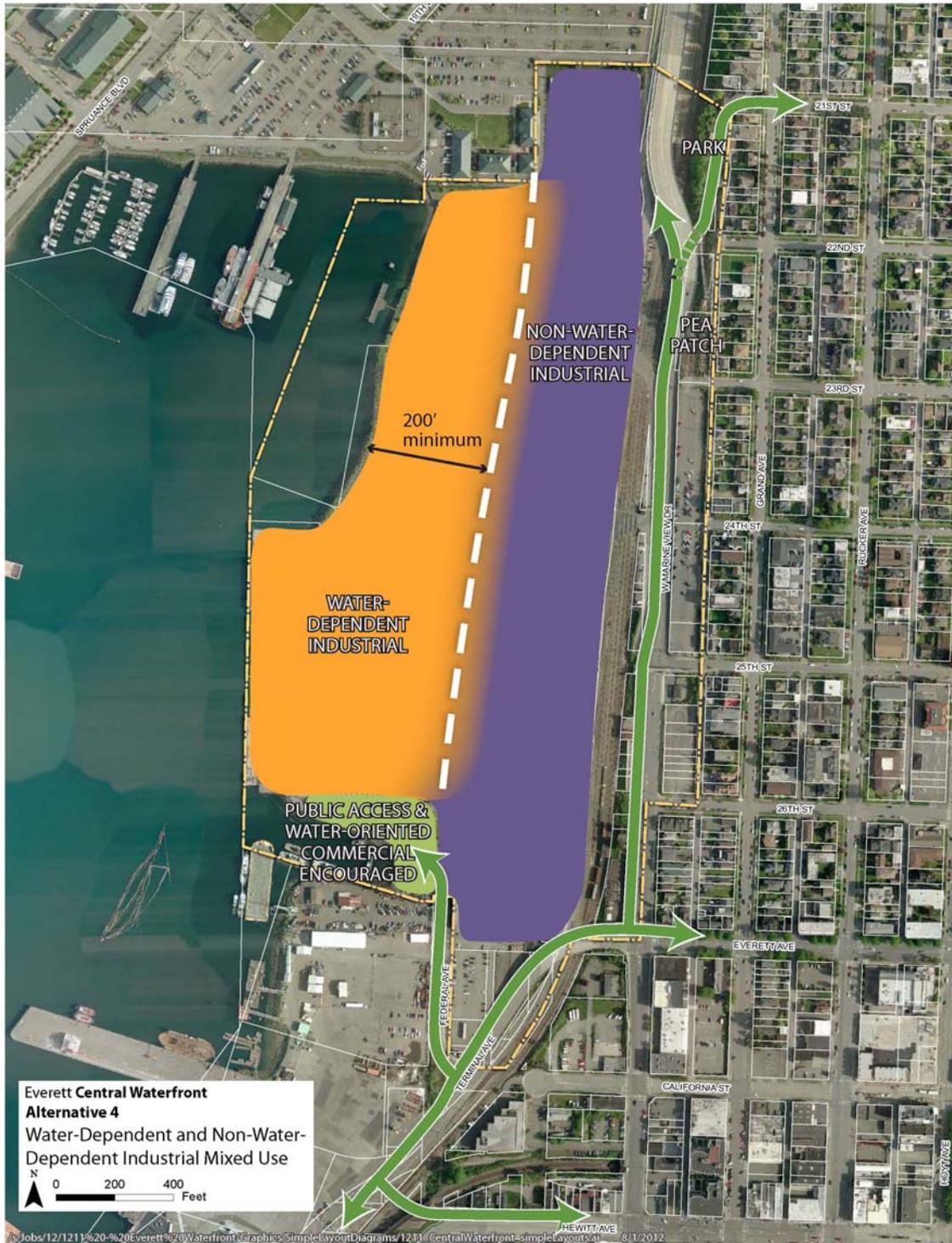
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Figure F



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Figure G



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- The provision of public access would result in an unavoidable health or safety hazard to the public that cannot be prevented by any practical means. The applicant must demonstrate that the health or safety hazards cannot be mitigated through the application of alternative design features or other solutions such as regulating access by such means as maintaining a gate and/or limiting hours of use; designing separation of uses and activities (e.g. bridges, pedestrian overpasses or underpasses, fences, terracing, use of one-way glazing, hedges, landscaping, etc.). Projects that meet these exceptions must construct off-site public access improvements or contribute to a fund to construct off-site improvements.
- The provision of public access would result in significant environmental harm, and the impact cannot be mitigated.
- The provision of on-site public access is not practical (e.g., small or odd-shaped lots, lots where functional requirements of the primary use would hinder access).
- More meaningful access that is better than that provided by the application of the goals, objectives and policies of this plan can be provided off-site.

Everett's SMP also requires that water-enjoyment uses and nonwater-oriented uses that front on the shoreline provide continuous public access along the entire site's shoreline. Everett's SMP includes a variety of standards for public access and requires that when a project is located within an area covered by an adopted public access plan, public access improvements shall be generally consistent with the adopted plan.

The City's SMP, adopted in 2002, and Shoreline Public Access Plan, adopted in 2003, did not anticipate redevelopment of the Kimberly-Clark site. The Kimberly-Clark site is designated Deep Water Port, so it would not be required to provide on-site public access under the current SMP, unless uses include water-enjoyment and/or non-water-oriented uses in the shoreline area. Water-enjoyment commercial uses are permitted in the Deep Water Port Environment and would be required to provide on-site access if proposed.

The specific public access improvements to be provided under any alternative will be established at time of review of shoreline permits based upon the nature of any use that is proposed within the 200 foot jurisdiction of the Everett Shoreline Master Program, the policies and regulations in this adopted plan, and the SMP requirements in effect at the time. The future use, operational safety needs, location of development on the site, and specific design measures will be evaluated to determine if on-site or off-site public access is more desirable or feasible. The improvements discussed below are conceptual. Before specific improvements can be constructed, additional planning, site analysis and design, and in some cases agreements with BNSF are required.

The description of alternatives above briefly states the concept for each alternative related to providing public access to the water. Under Alternatives 1 and 2, industrial uses in the Deep Water Port must be water-dependent or water-related, and on-site public access is not required.

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Alternative 3 would require amendments to the SMP to allow non-water-dependent / related industrial uses in shoreline jurisdiction and require on-site public access for those uses. This is shown conceptually as an open space at the south end of the water associated with a water-oriented commercial use and a trail along the entire shoreline edge. Alternative 4 encourages public access to be provided on-site, and also shows an open space at the south end of the site associated with water-oriented commercial uses. All of the alternatives show existing and potential pedestrian / bicycle connections to the eastern portion of the Kimberly-Clark site along Terminal Avenue and West Marine View Drive.

- The Shoreline Public Access Plan provides for the main trail link along the Kimberly-Clark site to remain on the sidewalk along the west side of West Marine View Drive. This connects to the Everett Avenue overcrossing and Port of Everett's Terminal Avenue/Pigeon Creek trail to the south. The Everett Bicycle Master Plan calls for the sidewalk north of Everett Avenue to be widened to 12 feet.
- A connection from the west end of Hewitt Avenue, beneath the BNSF mainline railroad and across the Bayside rail line, connecting the downtown and the central waterfront.
- A connection to Bayside Park under West Marine View Drive connecting the Bayside Neighborhood to the central waterfront area. This improvement, included in the Shoreline Public Access Plan and the Bayside Park Master Plan, would connect to the sidewalk trail along the west side of West Marine View Drive by the existing north gate / entrance to the Kimberly-Clark site.

There are a number of other potential public access improvements that could be implemented under the various land use alternatives. These improvements may include on-site improvements to complement existing pedestrian and bicycle facilities that exist in the area, or they could be entirely new improvements in another off-site location. Maintaining views of the water is also a major public access consideration.

On-site public access could include a wide variety of open spaces and trails along the water associated with water-oriented uses and views of industrial activities. See Appendix 5 for examples, including examples of public access provided in industrial areas.

Off-site access could occur in a variety of locations. Kimberly-Clark owns land and tidelands in the Snohomish River at Preston Point on the north end of the Everett peninsula. The Everett Shoreline Public Access Plan designates a shoreline trail through this property as part of the long term plan to develop a continuous shoreline trail along the Snohomish River (See Figure H).

Kimberly-Clark has not had any major shoreline permits issued since adoption of the current SMP in 2002. However, a 1993 Shoreline Substantial Development Permit for the Cogeneration Facility with Snohomish PUD No. 1 required construction of an off-site viewing platform along the bluff at 22nd Street, including dedication of 4 lots.

I. Proposed Land Use Regulations and Development Standards for Alternatives

The draft alternatives included a comparison matrix that spelled out changes to permitted uses and the potential changes to development standards under each of the 4 land use alternatives. A SEPA Addendum to the SEIS for the 10-Year Comprehensive Plan Update evaluated the impacts of the four alternatives and addressed potential mitigation measures to further reduce the impacts of development in the Central Waterfront Planning Area. This information was used by the Planning Commission to give staff direction for developing a Preferred Alternative. See Section 2 for the land use regulations for the Preferred Alternative.

J. Economic Analysis of Alternatives

The Economic Report prepared by Property Counselors (see Appendix 6) was developed prior to the development of the 4 land use alternatives. The City's consultant identified those uses that are likely to be economically viable on the Central Waterfront. Several economic concepts were identified that do not match precisely the land use alternatives described in this draft. However, all of the economic concepts evaluated in the economic report can be realized under the 4 land use alternatives. Rather than attempt to revise the economic concepts, this section will explain how they would be possible under the draft land use alternatives.

The staff / consultant team has developed 4 alternative land use / public access scenarios for consideration and evaluation for the planning process. The 4 land use alternatives do not match precisely the 4 economic concepts identified in the economic report. However, the analysis of the 4 economic concepts would generally be consistent with the land use alternatives as described below:

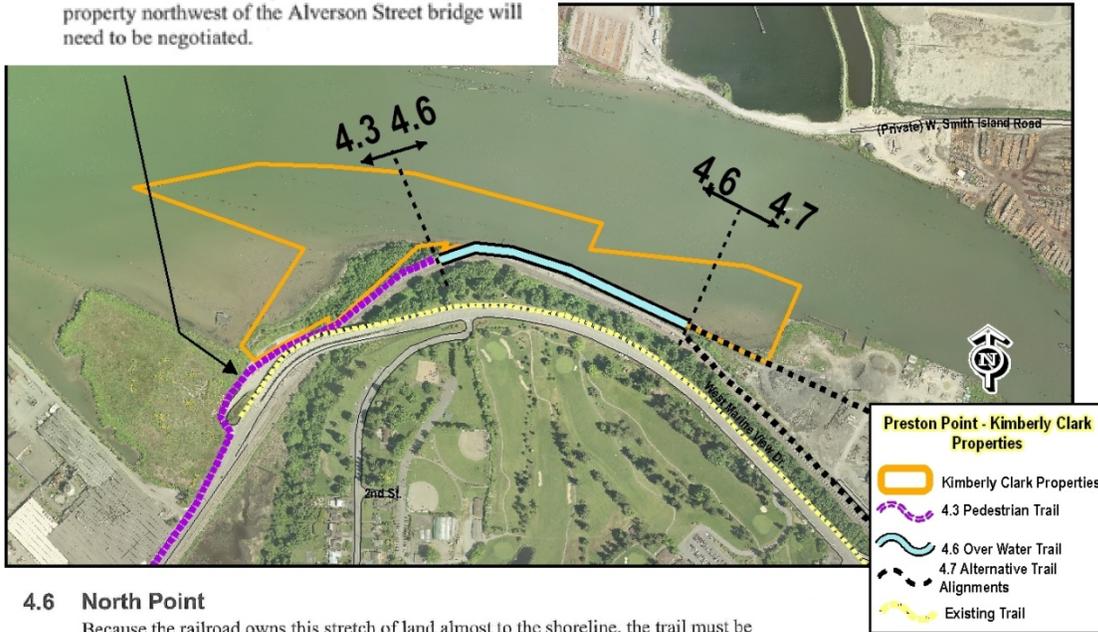
1. The "Water-dependent industrial use for the entire site" economic concept is very similar to both Land Use Alternative #1 (Existing Regulations) and Land Use Alternative #2 (Water-dependent and Heavy Industrial).
2. The "Non-water-dependent use on entire site" economic concept is very similar to Land Use Alternative #3 (Business Park and Public Access).
3. The "Mix of water-dependent and non-water-dependent industrial uses" can be developed under Land Use Alternative #4 (Water-dependent and Non-water-dependent Industrial Mixed Use).
4. The "Mix of water-dependent and non-water-dependent industrial uses / research / education / business park use" can be developed under either Land Use Alternative #3 (Business Park and Public Access) or Land Use Alternative #4 (Water-dependent and Non-water-dependent Industrial Mixed Use).

Figure H

Potential Public Access

4.3 Trail from the "Jeld-Wen" Site to Railroad Service Road

The existing walk should be repaired and pavement added where necessary. The access through the property northwest of the Alverson Street bridge will need to be negotiated.



4.6 North Point

Because the railroad owns this stretch of land almost to the shoreline, the trail must be aligned over the water. The projected addition of a new track will likely preclude use of the service road area. A pile- or fill-supported walkway 8 feet to 10 feet wide must include environmental enhancement with sufficient analysis and mitigation to ensure no net loss of ecological functions. The sections in Figure 11 illustrate the proposed approach. Kimberly-Clark currently owns the tidelands. The project would require the City to work with Kimberly-Clark to acquire the tidelands or a tidelands easement.

Figure 10. Example of a boardwalk in Poulsbo, Washington.

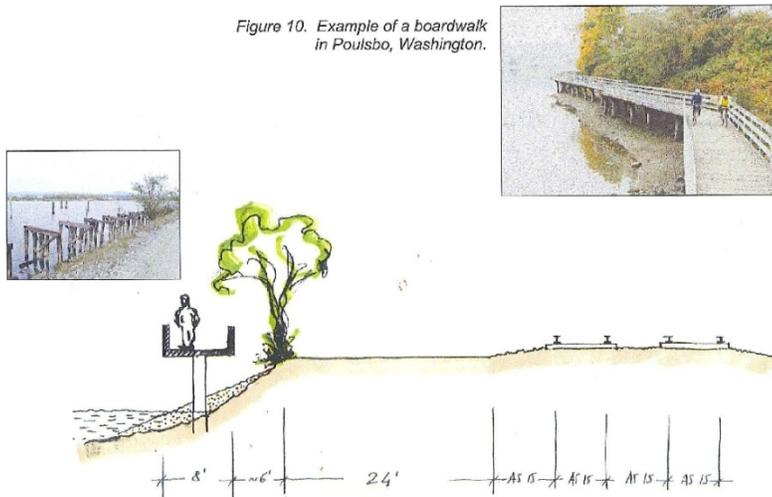


Figure 11. Proposed location and configuration of proposed boardwalk along North Point.

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The economic report states the following conclusions:

1. The most notable attributes of the site are its size and its deep water and rail access. These attributes make it most suitable for an industrial use that ships or receives freight by either water or rail. The location with water views and access, adjacent to Downtown Everett make it desirable for various non-industrial uses as well.
2. The viability of various potential uses of the property is related to the match of site attributes with user requirements, the long-term outlook for the use sector and the interest on the part of potential purchasers in response to the ongoing marketing of the property. These factors are summarized below.

	Outlook	Market Interest
Water-Dependent		
Cargo Handling	Strong	Strong
Shipbuilding	Strong	Moderate
Seafood Processing	Strong	Moderate
Non-Water-Dependent		
Energy/Environmental	Strong	Moderate
Water-intensive	Moderate	Low
Aerospace	Strong	Low
Other Manufacturing	Moderate	Low
Research/Education	Moderate	Low
Business Park	Moderate	Low
Commercial	Moderate	Low

The most viable uses are the water-dependent uses and energy-related. According to Kimberly-Clark, all of the interest expressed at the time of the economic report was by users rather than developers.

3. The cargo handling uses generally have lower employment densities than manufacturing uses or business park uses.
4. All of the sectors considered have high average wages.

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5. A research or education facility would be a strong anchor for an institutional or business park setting. Such users may require public funding, and no existing institutions are candidates at this time.
6. Four conceptual economic scenarios were developed to provide an illustration of how future land uses could be combined on the site and access provided for vehicles and possibly the public. While the land use alternatives have been refined, the four conceptual economic scenarios provide a continuing basis for evaluating economic benefits of the land use alternatives.
7. All of the candidate uses and site concepts would create economic benefits in the form of business gross receipts, jobs, wages and salaries, and local tax revenues. Impacts would accrue on a one-time basis during construction, as well as an ongoing annual basis with business activity. In addition to the direct impacts on the site itself, there would be indirect and induced impacts throughout the economy as local purchases and household spending work their way through the economy. The economic benefits do vary by concept. Generally, the non-water-dependent use concept has the highest beneficial economic impact across measures, with the water-dependent use the lowest impact. The mixed industrial use concepts fall between the two.
8. The Downtown would realize three specific economic benefits that differ among concepts. Spending in Downtown by workers at the site would be greatest for the concept with the highest employment density (Land Use Alternative #3). Further, purchases of goods and services by on-site businesses would be relatively higher for the water-dependent use concept because of its high employer multiplier (Land Use Alternative #2). Finally, the uses that would make the downtown most desirable to new residents and businesses are those that provide amenities and attractive views of the waterfront and beyond (Land Use Alternatives #3 and #4).
9. Overall, there is a trade-off between viability in terms of current demand and potential economic benefit. The Water-dependent use concept is the most viable in terms of current demand, but Non-water-dependent uses may offer greater potential economic benefit if such uses can be identified and attracted to the site.

K. Changes to the Comprehensive Plan or Shoreline Master Program

None of the land use alternatives would require an amendment to the land use designations of the City's Growth Management Comprehensive Plan. Of the four land use alternatives, only Alternative 3 (Business Park and Public Access) would require amendments to the Everett Shoreline Master Program and Comprehensive Plan policies. This alternative would require a

limited amendment to allow for non-water-dependent industrial uses within shoreline jurisdiction, and to require public access in the “deepwater port” shoreline use environment.

L. Preferred Alternative

Based upon the economic report, public comments, Planning Commission feedback, proposed site remediation and cleanup plan, and input from Kimberly-Clark, the Port of Everett and Naval Station Everett, the Preferred Alternative being recommended by City staff combines elements of the four Land Use / Public Access Alternatives and mitigation measures identified in the SEPA Addendum. See Figure I.

Following review, revision and recommendation from Planning Commission, City Council will adopt an ordinance that identifies specific uses and development standards that are different from current zoning. These revisions include prohibitions and greater restrictions on permitted uses, changes to development standards, establishment of design standards or guidelines that address site and building design, different review processes to allow for more public input in the permit process, and new criteria to be used in evaluating the potential for off-site impacts from permitted uses, and adds new information as follows:

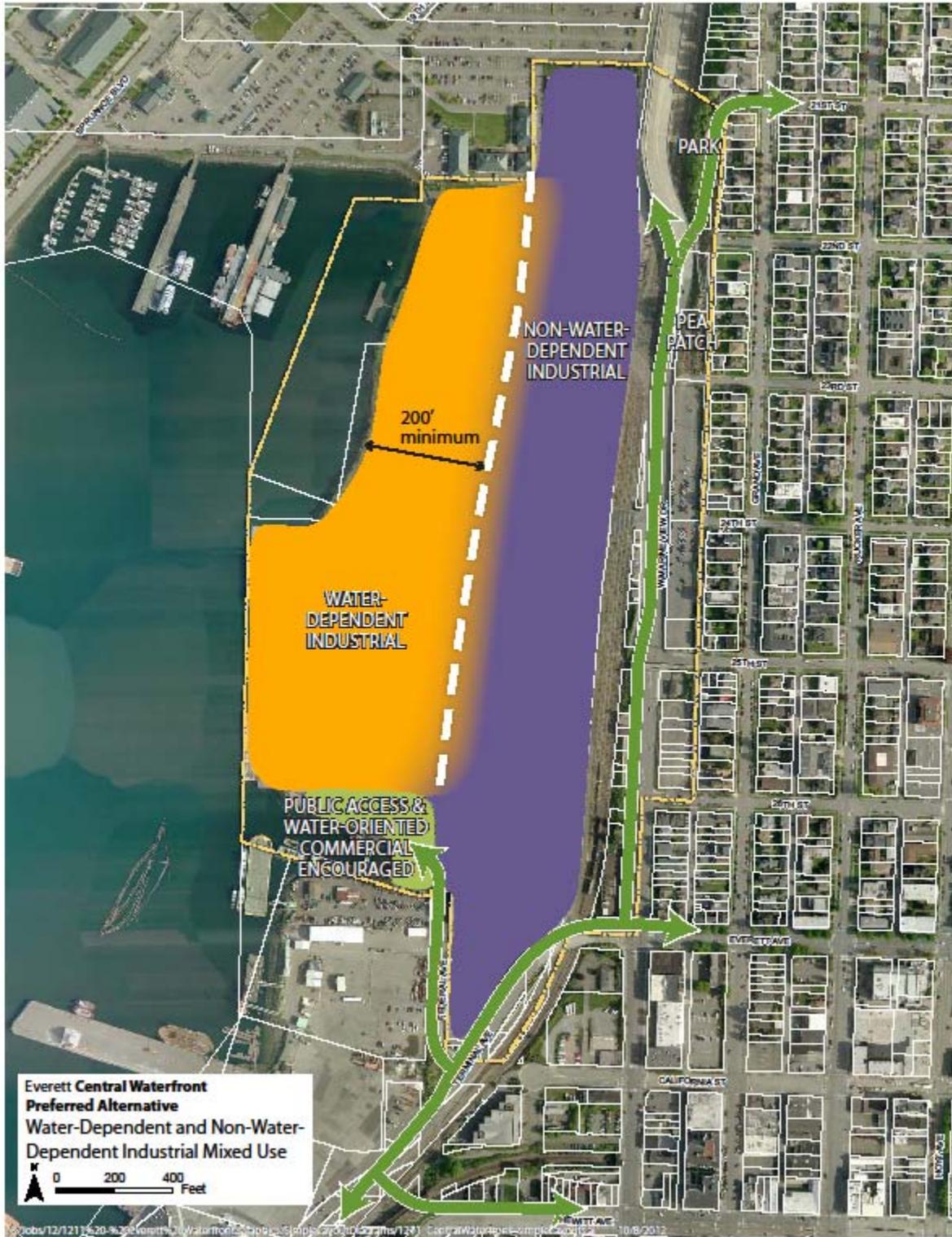
- **Land Use Vision: Properties west of the BNSF railroad right-of-way:** A future employment center primarily for water-dependent uses along the harbor with a mix of water-dependent uses and nonwater-dependent industrial uses outside of shoreline jurisdiction, all developed to high quality standards. Water-oriented uses with public access are encouraged. Development will be designed and operated to minimize impacts on adjacent residential areas, the downtown, and Port Gardner Bay. While not a land use requirement, in order to encourage investment and redevelopment in the adjacent community, the aspirational goal for redevelopment of the area is an average density of 10 or more jobs per acre.

At a future applicant’s request for amendments to the Shoreline Master Program, the City will consider allowing high quality nonwater-dependent business park type uses along the shoreline when public access and shoreline restoration are provided.

- **Public Access:** Public access is strongly encouraged on-site where not in conflict with water-dependent uses, and required on-site for nonwater-dependent uses in shoreline jurisdiction. Recommended improvements include a public viewpoint at the south end of the Central Waterfront Planning Area with connections to the downtown, and an enhanced pedestrian / bicycle corridor along W Marine View Drive. A pedestrian / bicycle connection from the west end of Hewitt Avenue to Terminal Avenue is a potential improvement that would substantially improve the connection between

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Figure I



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downtown and the Central Waterfront public access viewpoint described above. The viability of this connection should be further examined with BNSF.

If the site develops with water-dependent and water-related uses and public access cannot be accommodated at the site, public access should be provided elsewhere in the Central Waterfront Planning area shoreline, or where conflicts cannot be avoided, public access could be provided off-site, such as at the Kimberly-Clark property at Preston Point.

- **Properties between the BNSF railroad right-of-way and West Marine View Drive:** A commercial / industrial area with high quality development standards to implement the Comprehensive Plan designation of West Marine View Drive as a gateway corridor.
- **Residential properties east of W Marine View Drive:** No changes to existing regulations, except to provide for view corridors through street ends. The Bayside Neighborhood P-patch property will remain in use for a community garden or other public benefit use, unless changed to another beneficial community use, as determined by the City working with the Bayside Neighborhood organization.
- **B-1 property:** No changes

M. Economic Consultant Review of Preferred Alternative

The City's economic consultant has reviewed the draft land use regulations of the Preferred Alternative to determine whether it recognizes the economic realities for industrial uses and users, and whether it is overly restrictive and would limit productive use of property in the Central Waterfront area. His analysis of the permitted uses finds:

Permitted uses are identified subject to the further requirement that uses in the shoreline jurisdiction be water-dependent, an incidental part of a water-dependent use, or water-oriented. Prohibited uses otherwise allowed in the M-2 zone are also identified.

- The only obvious water-dependent uses that are prohibited are fish cleaning and processing, and coal export/shipping. Fish processing was identified in our report as a viable use, but would only occupy a portion of the site.
- The prohibited uses that aren't water-dependent and might otherwise be accommodated are the kinds of "low compatibility" uses that are typically strongly regulated in an urban area. No viable uses identified in our report are included in the prohibited list.
- A wide variety of manufacturing uses are permitted as well as business parks, and commercial uses serving other area businesses.

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The analysis of the Preferred Alternative also addressed the potential for on-site public access, impacts on the Port of Everett and to downtown, finding the following:

Public Access: Public access within the shoreline jurisdiction is strongly encouraged on-site where such access is not in conflict with water-dependent uses, and required on-site for non water-dependent uses. The regulations provide flexibility in providing public access in the shoreline zone without compromising operations on-site.

Impacts on Port of Everett and Downtown: Any new uses for the site would be incompatible with the Port if they interfered with Port operations. This is unlikely as the allowable uses and development regulations would provide for similar activities to what already occurs at the Port. As noted above, the public access requirements shouldn't compromise operations on the site itself and certainly wouldn't on adjacent properties.

As noted in our report, benefits to Downtown are greatest with a high employment density on-site and attractive views and amenities for Downtown residents and workers. The vision for high quality water dependent and non water-dependent uses with a goal of 10 or more employees per acre should result in such benefits to Downtown.

The economic analysis concludes: In summary, the preferred alternative recognizes the unique characteristics of the site, allows for viable uses, encourages high quality development, and will not limit productive use of properties in the area. Further, it should provide benefits for the Downtown without adversely affecting the Port. (See letter from Property Counselors dated October 8, 2012, in Appendix 7).