

**Instructions to reader:** This is a new chapter in the Unified Development Code. For a summary of the effect of this chapter from current standards, please visit the Rethink Zoning Library at <https://everettwa.gov/2453/Rethink-Zoning-Library>.

## Chapter 19.33 Streets, Sidewalks and Pedestrian Circulation

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#### **19.33.010 Applicability and user guide.**

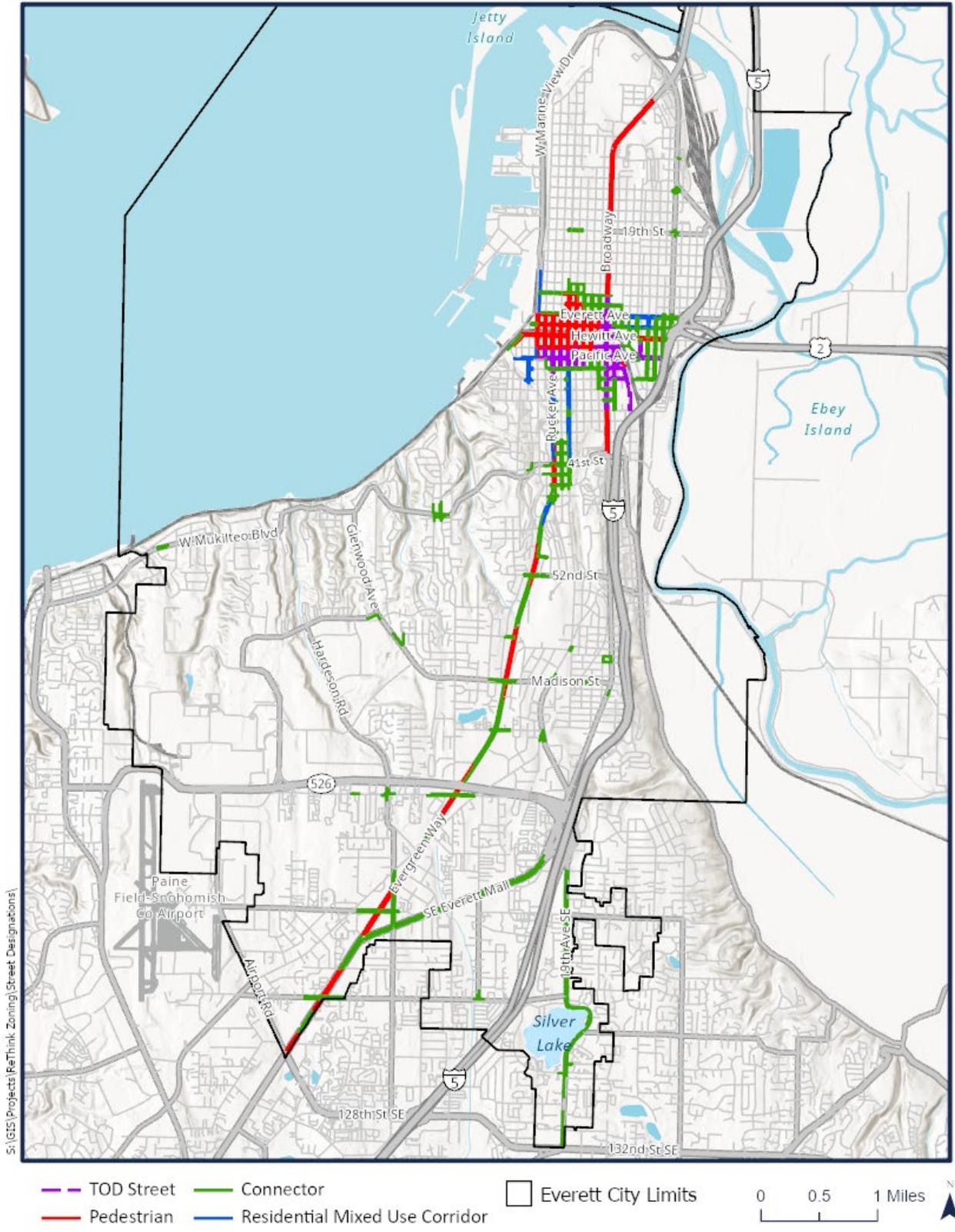
This chapter contains regulations concerning public sidewalks, pedestrian connections to public sidewalks, and internal pedestrian circulation within private property. It is intended to supplement the standards in Title 13, Streets and Sidewalks, and the City’s Design and Construction Standards and Specifications (DCSS). Detailed specifications and standard drawings for public streets and private access drives may be found in the DCSS. Where there is a conflict, the most restrictive regulations shall prevail.

#### **19.33.020 Street Designations.**

The designations established herein serve as a basis for application of development standards in the land use code. They are not to be confused with the functional street classification system used by the city, state and federal government for transportation planning.

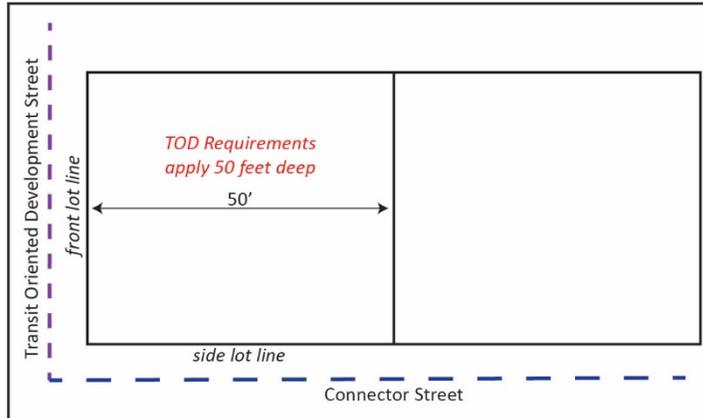
- A. Street Type Designations Map. Streets within Everett may have one of four street types designated: Transit Oriented Development Street (“TOD”), Pedestrian Street, Connector Street and Residential Mixed-Use Corridor. All other streets are considered “undesignated.” These street types function as a design and use overlay. For example, some uses may be prohibited outright along TOD or Pedestrian Streets, or the use may be prohibited on the ground floor of a building. Sidewalk, window transparency and other design standards may also be different based on the street designation. Use restrictions by street type are included in EMC 19.05. Please refer to EMC 19.12 for additional development standards for these street types.

Map 33- 1: Street Designations Map



- B. **Corner Lots.** For corner lots with more than one street type designation, the most restrictive street type designation applies to that portion of the lot measured 50 feet in depth from the lot line adjoining the designation. See Figure 33-1 for how to apply this requirement.

Figure 33-1: Corner Lots



- C. **How development standards are applied to specific properties based on street designations.** When a lot or lots have frontage on a public street with one of the street designations shown on Map 33-1, certain developments standards will be different than if the property fronted on a street without a street designation. The following standards are affected by street designations, as shown in Table 33-1:
1. Uses (EMC 19.05, Tables 5-1 through 5-5)
  2. Structured parking (EMC 19.12.110)
  3. Weather protection (EMC 19.12.120)
  4. Transparency (EMC 19.12.130)
  5. Public sidewalks (EMC 19.33.030)
  6. Minimum building height (EMC 19.22.020)

Table 33- 1: - Structured Parking, Weather Protection, Transparency, Sidewalk Requirements and Building Height by Street Type

Standard:	STREET TYPE DESIGNATION (see Map 33-1)				
	TOD	PEDESTRIAN	CONNECTOR	RESIDENTIAL MIXED USE	UNDESIGNATED
<b>Structured Parking Frontage Standards</b>	<i>See 19.12.110. Standards below are maximum distance a parking structure at the ground floor may occupy on various street designations.</i>				
Structured parking integrated with other building (accessory use)	10% of front building facade	25% of front building facade	50% of front building facade	50% of front building façade	50% of front building facade <sup>(3)</sup>
Stand-alone parking structure (principal use)	25 feet	25 feet	50 feet	75 feet	100 feet <sup>(3)</sup>
<b>Structured Parking Setback Standards</b>	<i>See 19.12.110.</i>				
Front, side and rear <sup>(1)</sup>	See underlying zone setback requirements and limits on frontage set forth above				
Below grade	0 feet				
<b>Weather Protection</b>	<i>See 19.12.120</i>				

Standard:	STREET TYPE DESIGNATION (see Map 33-1)				
	TOD	PEDESTRIAN	CONNECTOR	RESIDENTIAL MIXED USE	UNDESIGNATED
Length, minimum	90% of front building facade	75% of front building facade	45% of front building facade		Same width as entrance
Depth, minimum	8 feet from front building facade	6 feet from front building facade			3 feet from front building facade
Height above sidewalk	8 feet, minimum 15 feet, maximum				
<b>Transparency</b>	<b>See 19.12.130</b>				
Percent comprised of windows and/or doors with clear glass (2-10')	90%	60%	45%	45%	See blank walls (Chapter 21.040)
<b>Sidewalk Standards</b>	<b>See 19.33.030</b>				
Frontage Zone Width	2' min 6' desirable	2' minimum 6' desirable	None	None	None
Ped Clear Zone Width	8' minimum 10' desirable	8' minimum	6' min.	per city engineer	per city engineer
Landscape/Furniture Zone Width	6' minimum 8' desirable	6' minimum	6' min.	4' min.	per city engineer
Total Min. Width	16' min.	16' min.	12' min.	10' min.	6' min.
<b>Sidewalk Treatments</b>	<b>See 19.33.040</b>				
At least 2 treatments:	Required	Required	Required	N/A	N/A
<b>Minimum Building Height</b>	<b>See 19.22.020.B<sup>(2)</sup></b>				
Principal Building	2 - 4 floors	N/A	N/A	N/A	N/A
Accessory Building	No minimum	N/A	N/A	N/A	N/A

**Table 33-1 Footnotes:**

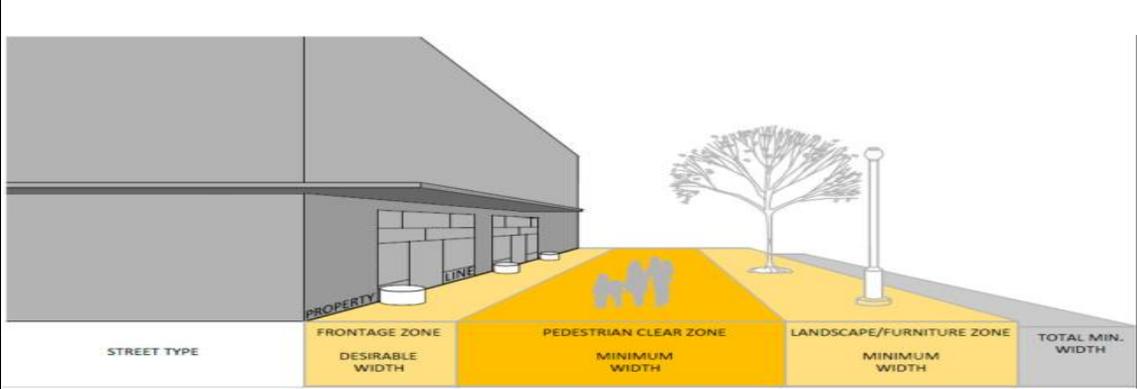
- <sup>(1)</sup> Structured parking garages accessed from the rear with no internal turnaround shall be setback at least 25 feet from the far side of the alley, except as otherwise approved by city engineer.
- <sup>(2)</sup> Refer to EMC 19.22 for additional building height standards.
- <sup>(3)</sup> Applicable only within Metro Everett.

**19.33.030 Public sidewalk requirements.**

- A. These standards apply to properties that front on a public street with a designation of TOD, Pedestrian, Connector or Residential Mixed-Use as shown in Map 33-1. The standards apply when a development is required to install street and alley improvements in accordance with EMC 13.68.020, or as amended. This includes both new development and expansions or alterations of an existing use.
  - 1. Public sidewalk standards for designated streets.
    - a. Sidewalks shall be installed on all frontage on public right-of-way, except when authorized within an easement by the city engineer.
    - b. Sidewalk width. Sidewalks shall provide frontage zone, pedestrian clear zone, landscape/furniture zone, and total minimum width consistent with Table 33-2.
    - c. Sidewalk pattern shall carry across the driveway.
  - 2. Undesignated streets. The standards in this subsection do not apply to streets shown as 'undesignated'. Sidewalk improvement standards for undesignated streets are based on the requirements in Title 13 and the city's Design and Construction Standards and Specifications.

- B. Historic overlay districts. Sidewalks in historic overlay districts shall include color and patterns similar to other sidewalks in the historic overlay as directed by the city’s public works standards.

Table 33- 2: Public Sidewalk Widths by Street Designation



STREET TYPE	FRONTAGE ZONE DESIRABLE WIDTH	PEDESTRIAN CLEAR ZONE MINIMUM WIDTH	LANDSCAPE/FURNITURE ZONE MINIMUM WIDTH	TOTAL MIN. WIDTH
	<b>FRONTAGE ZONE MINIMUM WIDTH</b>	<b>PEDESTRIAN CLEAR ZONE MINIMUM WIDTH</b>	<b>LANDSCAPE/FURNITURE ZONE MINIMUM WIDTH</b>	<b>TOTAL MIN. WIDTH</b>
TOD	2' – 6'	8' – 10'	6' – 8' 8' desirable	16' min
Pedestrian	2' – 6'	8' minimum	6' minimum	16' min
Connector	None	6' minimum	6' minimum	12' min
Residential Mixed-Use	None	Per city engineer	4' minimum	10' min
Undesignated	None	Per city engineer	Per city engineer	6' min

*See public sidewalk treatment requirements below for specific street designations*

**19.33.040 Public sidewalk treatments.**

On streets designated in Map 33-1 as TOD or Pedestrian, at least two sidewalk treatments are required from the following list:

- A. Special surfacing treatment, such as unit pavers, special materials, and inlays, as approved by the city;
- B. Artwork incorporated into or along the sidewalk which is approved by the city’s cultural arts commission;
- C. Decorative tree grates;
- D. Decorative clocks;
- E. Informational kiosks;
- F. Landscaping elements, not otherwise required by this title, incorporated into curb bulbs and/or sidewalks; or
- G. Other treatments as approved by planning director and city engineer.

**19.33.050 Exceptions to public sidewalk standards.**

The following exceptions to public sidewalk standards are allowed:

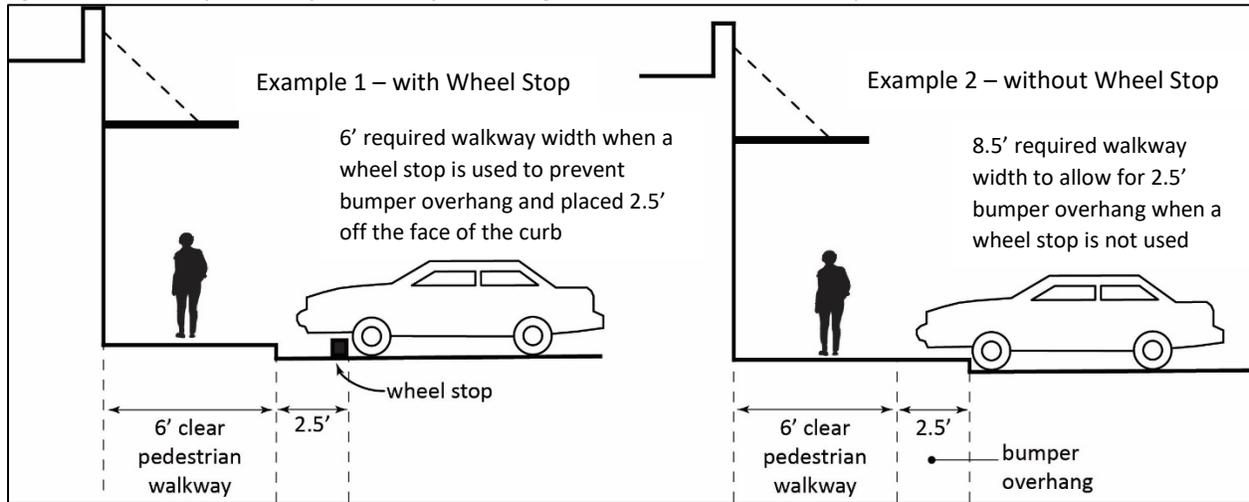
- A. Point obstructions: Point obstructions, such as power poles, light poles and fire hydrants, may encroach into the pedestrian clear zone but the sidewalk must have clear width remaining to meet accessibility requirements.

- B. Steep Topography at Right-of-Way Line: Sidewalks may be located adjacent to the curb when there is inadequate right-of-way or in steep topography areas where grading to a full street width would cause too great of an impact.
- C. Frontage and landscape/furniture zone shift: In areas where ground-level active uses are anticipated within the building frontage zone – such as sidewalk cafes or merchandise display – frontage zones should be designed to be wide enough to accommodate those uses. In no case can an active use encroach on the pedestrian clear zone. In rare cases, the furniture zone may be reduced in width when approved by the city engineer and planning director in order to maintain the minimum pedestrian clear zone and allow for activation of uses in the frontage zone.
- D. Other considerations at the direction of the city engineer. The city engineer may allow a modification or reduction of the public sidewalk standards in this chapter in the following circumstances:
  1. The required sidewalk improvements would conflict with existing underground or above-ground utilities, including sewer, water, stormwater, or power poles;
  2. A modification of the required sidewalk improvement is necessary to provide a smooth transition to an existing sidewalk on the adjacent property, or to an alley where the sidewalk intersects the alley; or
  3. The required sidewalk improvement would create a traffic or pedestrian safety hazard, such as sight distance problems or conflicts with other ingress/egress locations.

**19.33.060 Pedestrian access to public streets; internal pedestrian connections.**

- A. This purpose of this section is to provide safe and efficient pedestrian access from building entrances to the following: public sidewalks; transit stops; other buildings on the same site; parking lots; and common open space areas.
- B. Where required. An exterior pedestrian circulation system shall be required if there are no internal building pedestrian access routes that connect buildings to a public street, parking areas, or other buildings on the site.
- C. Design Requirements.
  1. All developments with exterior pedestrian circulation systems shall provide connections between building entrances and the public sidewalk, transit stops, off-street parking areas, common open space areas and alley where applicable. For developments with multiple buildings, provide for pedestrian circulation between all buildings.
  2. The exterior pedestrian circulation system shall be a minimum of three feet wide without obstructions, designed to meet federal, state and local accessibility standards, and where adjacent to driveways and parking areas they shall be separated by landscaping, raised curbs at least six (6) inches high, bollards, or other treatments as approved.
  3. For safety and access, landscaping shall not block visibility to and from a path, especially where it approaches a roadway or driveway.
  4. In commercial settings where buildings face onto a parking area rather than the street, provide six-foot wide walkways adjacent to the facades of retail and mixed-use buildings. This requirement applies where the adjacent parking stalls have wheel stops. When wheel stops are not incorporated, the walkway width shall be increased to a minimum of eight feet-six inches.

Figure 33-1: Walkway Width Adjacent to Surface Parking – With and Without Wheel Stops



5. Nonresidential and Multifamily Development: For nonresidential and multifamily developments with sixteen (16) or fewer parking spaces, the pedestrian circulation system may be located within an auto travel lane.
6. Single-family detached and duplex. A driveway may be used to meet the pedestrian connection requirement between the building entrance and the public sidewalk.

**19.33.070 Land Divisions for Residential development - pedestrian access to schools.**

- A. In all land divisions for residential development, the city may require pedestrian improvements to ensure safe walking conditions are provided between the development and existing schools or schoolgrounds.
- B. Pedestrian improvements may be required off-site in order to mitigate a safety hazard created by the development as determined by the city engineer.
- C. When a proposed division or redivision of land is on an established school bus route, the applicant may be required to provide a school bus shelter. The city engineer shall make this decision as it relates to the potential needs of the development.

**19.33.080 Easements and dedications.**

- A. In order to facilitate pedestrian access from a project to proposed and existing streets and to provide access to schools, parks, playgrounds, trails, transit stops or other pedestrian facilities, the city may require perpetual unobstructed pedestrian access easements of at least ten (10) feet in width to these facilities. For land divisions, pedestrian easements shall be shown on the face of the final division map.
- B. Where insufficient right-of-way exists to accommodate the full width of the required sidewalk, the city engineer may require either of the following:
  1. A public easement for the portion of the sidewalk located on private property; or
  2. A dedication of land sufficient to allow the sidewalk to be located entirely within the public right-of-way.