

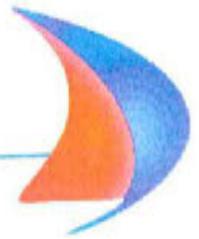
# Port Gardner Wharf Design Guidelines



**DRAFT**

Port of Everett  
Maritime Trust Company  
GGLO

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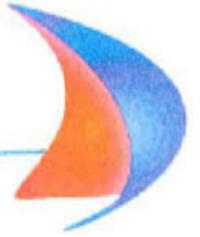
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Chapter I: Introduction

# Chapter 1A: Using the Design Guidelines

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Section A: Using the Design Guidelines

A.1 Applicability and Use of the Design Guidelines

Section B: Contacts & Acknowledgments



## USING THE DESIGN GUIDELINES

### Applicability and Use of the Design Guidelines

#### Intent -

The Port Gardner Wharf Guidelines are a tool for communicating the expectations for Port Gardner Wharf's character to designers, engineers, developers, investors, jurisdictions, and the public. The guidelines include images, suggestions, standards, and requirements to help convey, define, and plan the future for this important new community at Everett's waterfront. Some guidelines are general in nature to inspire creativity and allow flexibility, while others convey very specific standards. Together, their intention is to create a new community that has both a strong identity and a rich and interesting diversity between districts.

Design is fundamental at creating the unique character of place at Port Gardner Wharf. A Design Team's planning process, design, and construction documents should evolve from the careful and thorough analysis of the site and the building's character, setting, and features. Owners, Architects, and design team consultants must approach a project with an understanding and regard for the existing and future features and constraints.

#### Users -

These Guidelines are intended to be used by:

- Design Development Teams including Developers, Architects, Urban Designers, Landscape Architects, Artists, and Engineers.
- City and Port of Everett

#### Organization of the Design Guidelines -

Organization of the Design Guidelines document parallels the development process. Topics begin with the broad picture of the Design Context and moves through the Public Realm, and into Building Design. Design Context (Chapter 2) defines the area, the master plan layout, the established design Districts, and site wide environmental design features such as place making, art, and signage. The Public Realm (Chapter 3) defines

public areas and amenities including streetscapes and the pedestrian network, plazas and open space, and the water's edge esplanade and marinas. Building Design (Chapter 4) covers the building parcels through site design, general building standards, and standards specific to the various building typologies.



Chapter I: Introduction

# Chapter 1B: Contacts and Acknowledgments

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Section A: Using the Design Guidelines

Section B: Contacts & Acknowledgments

B.1 Contacts

- MTC
- Port of Everett
- City of Everett
- GGLO

B.2 Acknowledgments

- City of Everett
- Port of Everett
- MTC
- GGLO



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## **ACKNOWLEDGMENTS**



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### **Port of Everett**

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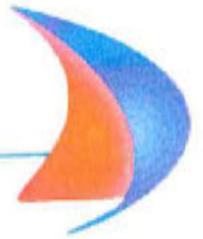
Tom Drugan, Design Guideline Lead

Sean Canady, Mark Sindell, Mieke DiPippo, Shaney Clemmons, Mark Wainwright, Kristin Ford

Chapter II: Port Gardner Wharf Design Context

## Chapter 2A: Site & Goals

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Section A: Site & Goals

A.1 Introduction

A.2 Development & Community Goals

A.3 Site Context

- Site Location
- Site Access and Circulation
- Views and Visibility

Section B: Port Gardner Wharf Master Plan

Section C: Environmental Design



Port Gardner Wharf Site

## SITE & GOALS

### Introduction

The following Port Gardner Wharf Design Guidelines are intended to be administered and approved by the Everett Planning and Community Department as a complimentary to the Planned Development Overlay (PDO) Ordinance and Development Agreement. The Guidelines present a range of design solutions and approaches to achieve a high quality implementation of the development goals. These goals focus on making Port Gardner Wharf a pedestrian oriented, mixed use development with extensive public access and open space.

The Guidelines will be used to guide site development based on the preferred site master plan developed by the Port of Everett and Maritime Trust Company. The Guidelines are intended to create a high standard of site function, appearance, and public access. The guidelines and standards provide a baseline of quality that will foster, rather than preclude or discourage creative or innovative solutions and approaches. Built-in flexibility will ensure that equal or better solutions than those depicted can be explored by development teams.

Port Gardner Wharf will be transformed from its traditional use as a "working waterfront" to a new and vibrant waterfront destination community. Although maritime commercial services will remain, they be consolidated north of 13th Street with a new boat launch and haul out ramp. This consolidation is making way for new retail, office, and residential uses, as well as the addition of significant public spaces and access. A new Amphitheater and a public Esplanade that wraps the

wharf's entire water's edge are major components of the public access improvements. Another important new amenity is the development of a new marina on the north side of the wharf in the 12th Street Waterway. On the other side of the wharf to the south is the 14th Street Marina (formerly called the North Marina), which is under going major improvements.

**Development & Community Goals**

The following goals represent the City, Port, MTC and community's vision for Port Gardner Wharf.

- *Create a pedestrian friendly waterfront destination.*
- *Provide public space and access to the waterfront.*
- *Link the City of Everett to the waterfront with connections to Port Gardner Wharf.*
- *Maintain and enhance view corridors.*
- *Develop a mix of uses for a diverse and vibrant community.*
- *Retain some existing maritime uses.*
- *Create strong site and building interrelationships.*
- *Integrate Sustainable Design Practices to create a healthy community.*



A Waterfront Community and Destination



Mix of Uses



Waterfront Access



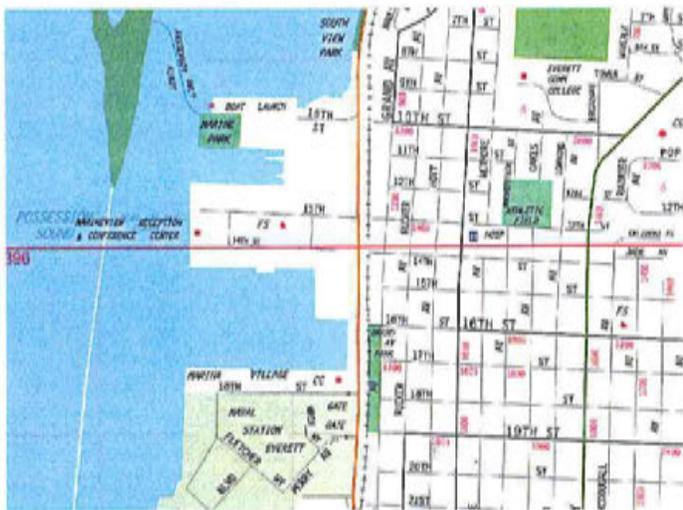
Quality Architecture



Public Open Space

## Site Context

**Site Location** – Port Gardner Wharf is centrally located at the shoreline of the City of Everett waterfront, tucked between a residential neighborhood on a bluff to the east and the Snohomish River Waterway on the west. Separating the Wharf from the Puget Sound is a thin sand bar beach known as Jetty Island.



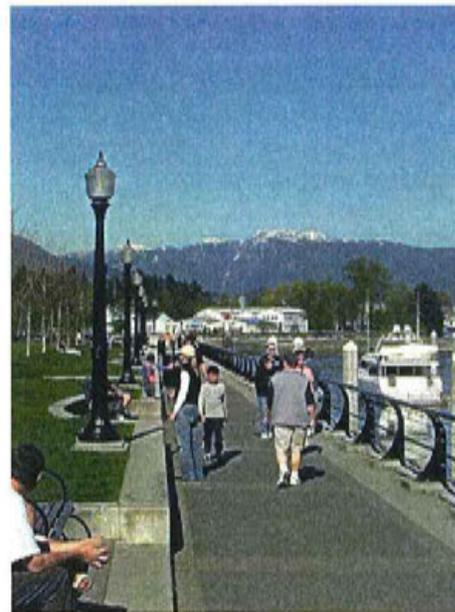
Site Vicinity

**Site Access and Circulation** – Port Gardner Wharf will be pedestrian oriented community neighborhood with extensive public access and open space. Streets, sidewalks, and walkways provide the fundamental structure for access and circulation for pedestrians, vehicles, buses, and bikes. They knit together the internal linkages between areas and allow vehicles and pedestrians to be able to flow smoothly and safely through the site. Site access and circulation at Port Gardner Wharf will balance vehicular, bike, and pedestrian access. All must be clearly visible from adjacent streets and to the extent possible they should provide some separation and definition.

**Pedestrian Network** – A continuous pedestrian network shall create safe access around and through Port Gardner Wharf.

North-south pedestrian corridors include the west side of West Marine View Drive, the 14th Street marina to the 12th Street Marina on the north, through the residential

area, and another along the Esplanade. East-west connections include the south edge, 14th Street, 13th Street through the center of the wharf, and along 12th Street. The pedestrian corridors will include connections to the Water's Edge Esplanade. The Esplanade is a major promenade wrapping the site. At 20' wide, it includes a 12' minimum clear travel zone for walking and biking. An additional 10' will be used to transition to upland landscape and buildings.



Pedestrian Access at the Water's Edge

Pedestrian safety will be important. In addition to meeting all City, State, and Federal requirements for pedestrian safety, a series of guidelines for enhanced cross-walks and well defined crossing points are included. Walkway materials and special textures and colors will cross vehicle travel lanes where cross walks are provided. In some areas, intersections may also be elevated to further emphasize the crossing area for safety.

**Bicycle Access** – Port Gardner Wharf will connect with the City-wide bike system via a connection from West Marine View Drive along 10th Street to the multi-use waterfront Esplanade. The Esplanade wraps the entire site and connects back onto W. Marine View Drive at the southern end.

**Vehicle Access** - Streets provide both a unified network for access as well as an important element in creating the character at Port Gardner Wharf. A network of streets with varying size and function establish a hierarchy based on use, traffic and pedestrian volumes, location, and linkages. There will be commercial streets, neighborhood residential streets, alleys, service lanes, mews, parking aisles, and the 13th Street Boulevard.

The variation and specificity of the street types reflect the unique quality envisioned for specific districts at the Port Gardner Wharf. Within each district, streets will take on an identity that reinforces that district's distinctive character established through urban design and streetscape standards and guidelines.

**Visibility** - Design for safety, security and character of place will include maximizing visibility. Visibility should be maintained in all areas by eliminating high obstructions such as tall walls and fences that act as visual barriers. Ground level lighting is required along all walkways to increase safety and security at night.



Port Gardner Wharf View across the Puget Sound

**Views** - Maintaining and enhancing views is a paramount goal for Port Gardner Wharf. The community and visitors will enjoy the views and quality of place the marine environment provides.

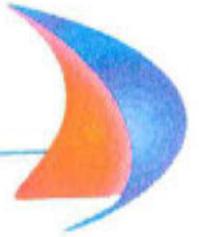
**View Corridors** - View Corridors at 13th and 14th Streets will preserve and enhance existing views of the Puget Sound and marina. The view corridor shall be protected by limited building along these corridors in accordance with Port/ City development agreement.

**Residential Views** - Development will be sensitive to views from hillside residents to the east by height limit, high quality design, and by providing roof amenities such as planted balconies and rooftop terraces. Mechanical equipment will be screened and roof top color will be selected to minimize glare. Views from new residential buildings on the site will be protected and enhanced by a series of design measures covered in these Design Guidelines.

Chapter II: Port Gardner Wharf Design Context

# Chapter 2B: Port Gardner Wharf Master Plan

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Section A: Site & Goals

Section B: Port Gardner Wharf Master Plan

B.1 Preferred Site Master Plan

- Land Use
- Phasing

B.2 Districts

- Gateway District
- Craftsman District
- Captains Walk/ 12th Street District
- Residential Neighborhood
- West End
- Water's Edge

Section C: Environmental Design

## PORT GARDNER WHARF MASTER PLAN

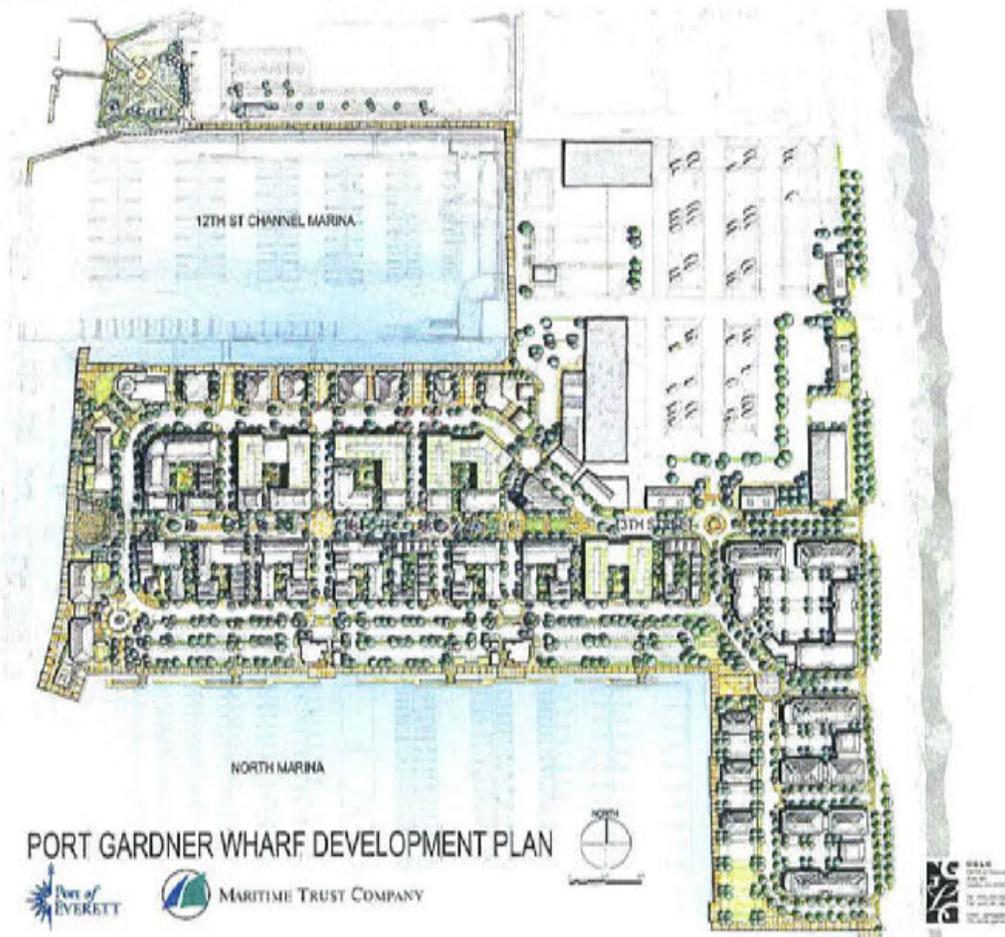
### Preferred Site Master Plan

A preferred site master plan has been developed for the Port Gardner Wharf. Developed from previous site design alternatives for the "North Marina" project as it was previously called, it reflects community input and the needs of the Port of Everett and Maritime Trust Company. It preserves the established use options, view corridors, and circulation system, while maintaining flexibility in phasing. The master plan of the site includes a collection of Districts that define unique areas and uses throughout the site. Location of district borders could shift, but the general layout will not substantially change.

**Land Use** – Port Gardner Wharf will have a Waterfront Commercial Comprehensive Plan designation. Rezoning of the site from Maritime Services (M-S) to Water-

front Commercial (W-C) is implemented through a Planned Development Overlay zone (PDO). Future uses will include not only the existing maritime businesses and operations, but also a wide range of public amenity, commercial, and residential land use.

**Phasing** – Phasing begins with shifting some of the existing maritime uses to the Craftsman District. Some of these uses will be accommodated with new facilities. As these relocate, infrastructure improvements such as 13th Street will begin and lay the foundation for the Phase 1 development. Phase 1 will be clustered around the Captain's Walk District and also include portions of the Water's Edge. Subsequent Phases move toward the west end of the site, building out the Neighborhood Residential area, the 12th Street Retail area, and the West End. The Gateway District will come in the final phases.



Port Gardner Wharf Conceptual Site Master Plan  
(Conceptual Illustration Only - Final Plan May Vary)

## Districts

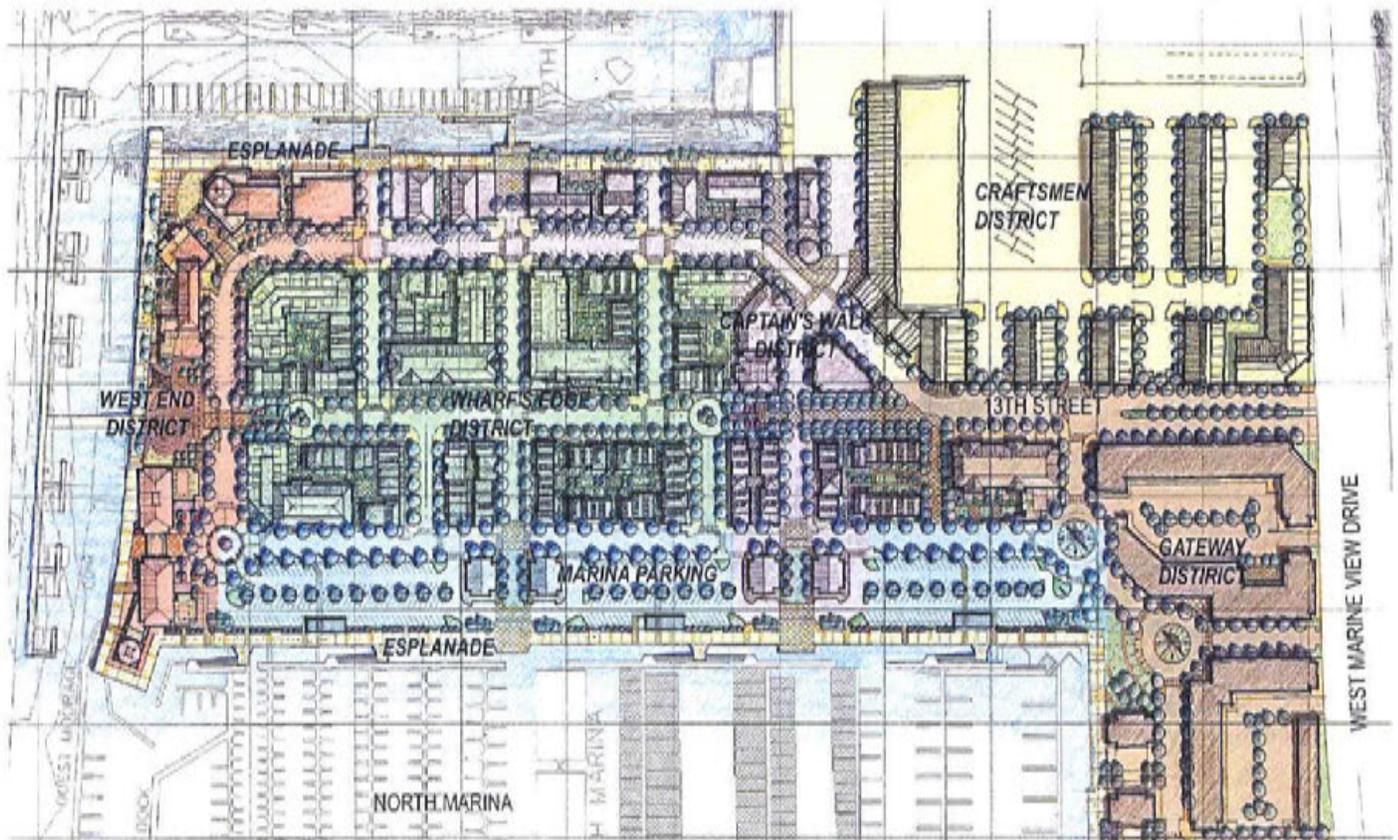
Port Gardner Wharf is a collection of unique Districts that together convey their common maritime sense of place. District characteristics will reflect unique defining traits according to their primary function, but also be coordinated through design to add to the site's overall richness. Common uses between different districts will allow district characteristics to reinforce each other's qualities, while uses unique to a particular district will allow distinction. Within each district, design will take on an identity that reinforces that district's distinctive character.

District identities, described below, will be established through urban design elements and through building design covered in the guidelines and standards throughout this document. Urban design improvements are covered in Chapter 3 The Public Realm which includes guidelines on streetscapes, plazas, and the water's edge. Design of buildings and their sites are in Chapter 4.

**13th Street Gateway District** - The Gateway District area along the south eastern portion of Port Gardner Wharf runs adjacent to West Marine View Drive from 13th Street to the south property line. It contains the main entry to the Port Gardner Wharf at 13th Street and a secondary access at or in proximity to 14th. 14th and other streets will primarily serve to access the future development in this area which may include larger commercial office uses. Design character will be less traditional forms expressed with clear geometries and a rich palette of materials. Within the district, the 13th Boulevard Entry



13th Street Gateway



Port Gardner Wharf Districts

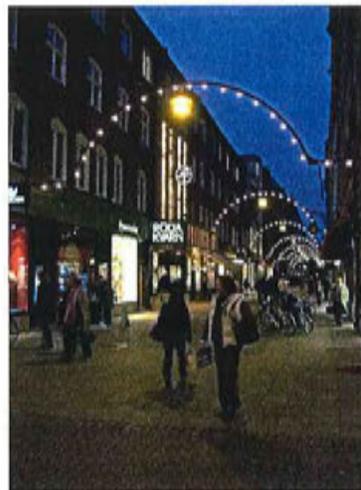
should have a distinction from the Gateway District and display the unique and evocative quality of Port Gardner Wharf through its urban design treatments. Building and street frontage along W. Marine View Drive will provide an attractive, active, and interesting edge as a community face and gateway into Port Gardner Wharf.

**Craftsmen District**- Craftsman District includes areas north of 13th Street and east of 2nd Avenue from the future boat launch to West Marine View Drive. The area will serve marine related activities and services. This area includes all facets of commercial marine activity including the Marine Commercial, Retail, and Light Industrial marina support activities. Here, boats are sold, moved, washed, worked on and stored. There will be travel lifts, air tools and other work related activities during most of the working day. Design will draw on the craft, technology, and history of the working waterfront and reflect that quality with durable maritime materials and forms, and expressive structural details and connections.



Craftsman District

**Captain's Walk/ 12th Street District** - The Captain's Walk/ 12th Street District is the retail heart of Port Gardner Wharf. As the central retail area, it will include cafés, shops, and streets that are active and vital. The District is focused around 12th Street and 2nd Avenue, a pedestrian street linking the 14th and future 12th Street Marinas. On 12th, there will be commercial retail associated with the development



Captains Walk District

along the Esplanade. Along the Captain's Walk, the Market/ Festival Plaza will be designed as a pedestrian amenity that will enrich this district's unique character. To express the character and quality, the District will use custom features and more ornate details that express a rich palette of materials. The elements will be evocative of the site's maritime heritage with nautical inspired forms and details and with rich colors and materials.

**Wharf's Edge Residential Neighborhood** - The Residential District takes up the large central western portion of the Wharf inside the 200 foot shoreline management zone. This area will be predominantly a quiet multi family residential area with the potential of some retail and office on the ground floor at main corners. Residential courtyards, pocket parks, and the 13th Street 'Ramblas' pedestrian route will help to create a strong community identity in this district. The character will be expressed through classic materials and traditional decorative detailing and forms. Diversity of housing types will add to the mix and vitality of the new neighborhood.



Residential Neighborhood

**West End District** - The West End includes Restaurant, Hospitality, and Amphitheater areas that will define the western end of the Wharf along the Snohomish River Channel. Design elements may be less ornate than the retail area with bolder forms and a scale that melds



West End includes Major Public Open Space



rather than distracts from water views and from the unique character of the restaurants and hotel. Access to the businesses and public Amphitheater will be off of 6th Avenue which defines the eastern side of this district. The western edge will connect seamlessly with the Amphitheater and provide easy access to the Esplanade and a tall ship moorage.

**Esplanade/ Water's Edge** - The Water's Edge District is the focus for public access including the Esplanade, Marinas, and other associated transitional places bordering the entire Port Gardner Wharf development. The Esplanade is a great pedestrian amenity that will provide full public access to the shoreline. Plazas and view points will enhance upland connections to the Water's Edge District. Design will reflect the maritime quality with expressive forms and details inspired by the sleekness of boats, ships, and nautical equipment

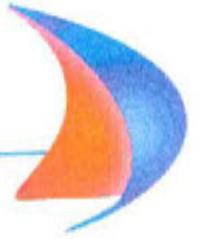


Water's Edge is the Focus for Public Access

Chapter II: Port Gardner Wharf Design Context

# Chapter 2C: Environmental Design

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Section A: Site & Goals

Section B: The Master Plan

Section C: Environmental Design

C1 Place making

C2 Art and Other Special Features

- Intent
- Materials and Finishes
- Types of Art

C3 Signage Guidelines

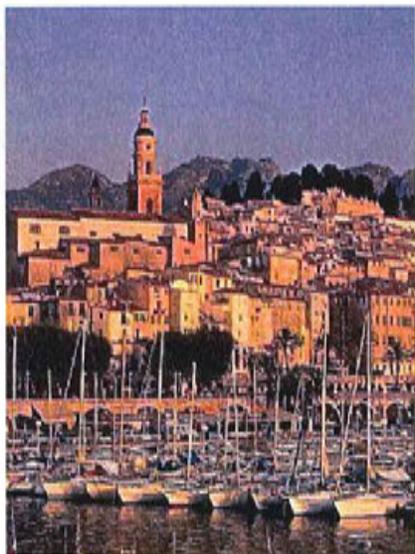
- Introduction
- Sign Review
- Sign Types
- Sign Sizes

## ENVIRONMENTAL DESIGN

### Place Making

Port Gardner Wharf will be more than a collection of buildings and amenities, it will become a unique and special place that is a living and dynamic community. Place making is a complex process with many elements of which environmental design plays an important role.

Environmental design creates the basic building blocks of place-making through the exterior architecture of buildings and the urban design and landscape architecture of sites and public spaces. Successful place making weaves together the architecture and site creating a dialog between public and private realms. A vocabulary of materials, forms, and details come together in exterior elements such as building components, lighting, paving, and planting to form the character of place.



Place making Integrates Building and Site

At Port Gardner Wharf, these elements will combine and coordinated to create a strong overall community identity, but also have variation that helps accentuate the uniqueness of the various Districts. These design elements are covered in Chapters 3 Public Realm and Chapter 4 Development Guidelines.

Special environmental design features such as art and signage, come together to further enhance the quality of place. Art, signage, and other special elements have the greatest potential for powerfully expressing some unique qualities beyond the building and it's site. These elements, covered here in Chapter 2, will make the place where they are truly unique and memorable.

### Art and Other Special Features

#### Intent –

Port Gardner Wharf provides many opportunities for art and artful water features, monuments, and interpretation. These special features help create the one of a kind sense of place that will enrich the experience and memory of being at Port Gardner Wharf.



Waterfront Sculpture

Art, when done well, can transmit, expose, and enhance a unique emotional response to special places. A variety of works should be explored to add a dimension of place-making that can be contemplative, engaging, or fun. Art can provide opportunities for discovery, or simply add a sense of whimsy. Like other features of Port Gardner Wharf, the art should tap into the uniqueness of the site and its connection to the maritime environment in order to add to the community's unique waterfront sense of place. Art that explores maritime themes and site specific natural and/or cultural meaning is well suited.

Water features, monuments, and interpretation are other artful features that convey and create unique experiences for the community and for visitors. Like art, they have an aesthetic content that will add special and memorable qualities to areas and districts within Port Gardner Wharf.

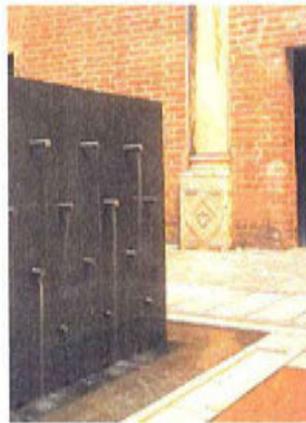
Because art is an integral component to place making, art or artful components are required for all significant buildings or site related projects. The use of Pacific Northwest artists is strongly encouraged.



Art is Vital to Place Making

## Materials and Finishes –

Exterior art and other special features must be made of materials that are durable in the marine climate. This includes a variety of masonry including stone or concrete, as well as various metals such as stainless steel, heavy gauge aluminum, or bronze. In some cases, such as with interpretive panels, the art may also include some highly durable and vandal resistant thermoplastics. Where light and sound are used for art, the housing and fixtures must be weather and vandal resistant. For all art, finishes must be long-lasting and low maintenance. Sealants should be considered in areas with potential exposure to vandalism or staining.



Materials Must be Durable

## Types of Art and Other Special Features

**Sculpture and Found Objects –** Sculpture or a powerful found object can create a focal point within a space or view. Large found objects such as ship anchors, propellers, or other industrial marine related pieces can be used as found for large place making iconographic objects.

**Integrated Art–** Integrated art and artist made building parts can tie art and art concepts to functional elements and spaces. Scale can and should vary widely from small touches of art to large scale environmental works. Smaller elements may be pieces embedded in walls and walkways or building components such as brackets, canopies, lights, downspouts, floor patterns, and signage. Large scale environmental



Use Found Maritime Objects

works will explore concepts in the open space areas. These may look at spatial phenomena and landscape as an aesthetic palette. Larger scale works could include an artist designed plaza, waterfront installation, or water feature. On large projects, an artist should be on the design team for their work and ideas to be integrated with the overall design of the building or site.



Small Scale Integrated Art

**Water Features –** Fountains, jets, misters, and water channels create strong sensory spaces as well as playful places for interaction. Water features are encouraged in public plazas to add interest, activity, and stimuli.



Interactive Water Features

**Monuments –** There may be a desire for the addition of monuments. Monuments can mark a site of historic significance, or memorialize a historic event or person. Monuments should fit within their context, be integrated into walkways or walls, and convey their meaning in simple text or imagery.



Monument

**Interpretation–** Like monuments, interpretive markers can demarcate a site of historic or natural significance. Unlike monuments, interpretation builds a thematic topic and narrative to uncover the site's natural or cultural attributes. Interpretation should be used sparingly at Port Gardner Wharf and integrated into walls and railings so as to not impede views.

## Signage Guidelines

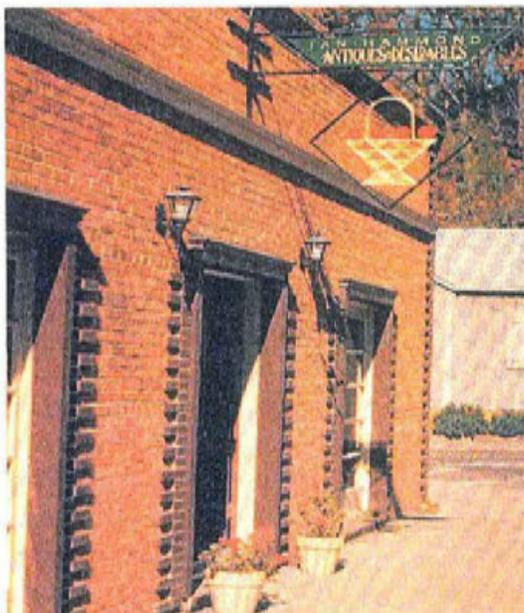
### Introduction -

Signage is another important element in place making. Well-designed and interesting signs will establish a distinct and memorable identity for Port Gardner Wharf. The overriding concept is to achieve uniqueness within the individual businesses while at the same time create a cohesive district identity that is harmonious with the overall character of Port Gardner Wharf.

Designed signage is encouraged to be interesting, creative, and unique in both its design and use. They should be artistic, graphic, expressive, and individualized, and yet tie into the marine themes and district identity that will be part of Port Gardner Wharf. Signage will be integrated with architecture and will be part of the overall design approach of the district's place making rather than being seen as a separate design statement.

### Sign Design Program and Review –

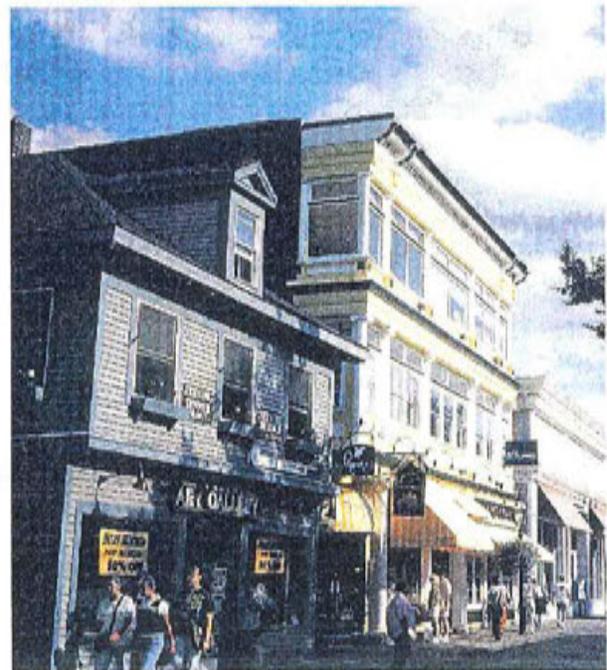
The design of buildings and sites shall identify locations, sizes, and conceptual character for future signs through a 'sign design program'. The sign design program (or sign program) is required to ensure signs fit with the



Signage Integrated with Architecture

architectural character, proportions, and details of the building in concert with the district and community identity of Port Gardner Wharf. As tenants or owners install signs, it is expected that such signs shall be in conformance with the approved sign program.

Development teams are required to review and meet the intent of these design guidelines through the sign design program. The sign design program will use text, scaled drawings or graphics, and examples to identify locations, sizes, and conceptual character for future signs.



Signage Adds to the Street Character

### Sign Types –

**Traffic, Regulatory, and Safety Signs –** Signs such as traffic directional signs, regulatory signs, speed limit signs, pedestrian safety signs, parking enforcement signs, ADA signs, fire safety signs, etc. shall be designed and placed in accordance with City of Everett, and the State of Washington regulations and requirements.

**Street Name and Direction Signs –** Street name and direction signs provide an opportunity to reinforce the Port Gardner Wharf character. Design must use con-

sistent graphic elements such as font type, style, color and size, but should draw on elements of uniqueness to help reflect the quality of each district. These elements of uniqueness could include the sign frame, stand, base or perhaps the addition of an artistic graphic. Text must be in strong contrast to its background and be large enough for clarity from moving vehicles at a distance sufficient enough for safe cueing and turning.

Street Address Signs – Buildings will identify their address through street address signs located near the building’s main street entry. Design will be compatible with architecture and district character.



Direction Signage

Business and Residential Identification Signs– Identification is vital to the success of any retail or commercial venture. Signage should be used to imaginatively punctuate the business’s unique quality. This will also create interest and vitality along the street.

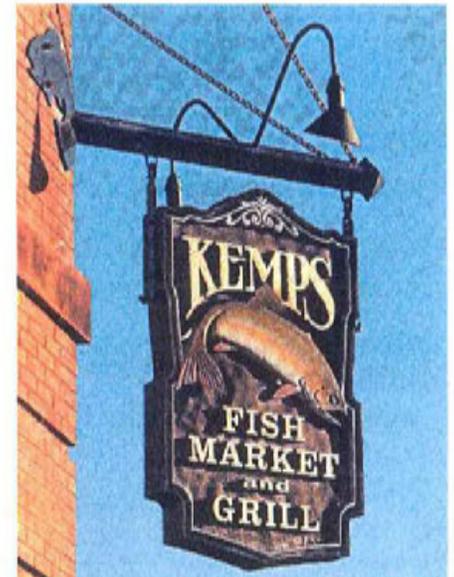
Interesting colorful and tastefully designed signage will inform, stimulate, and identify the business, building, or retail tenant. Graphics and symbols that rely on images and reduce the use of words are encouraged. Permanent lettering and graphics on windows and doors may be used in lieu of sign boards.

Commercial signs will be complimentary to buildings, and will communicate the availability of goods and services in a lively, creative manner to both pedestrians and vehicles. They will contribute to the character of the Port Gardner Wharf. In pedestrian areas, all signs shall be mounted on buildings. These include projecting signs, wall signs, window signs, signs painted on awning fascias, and signs suspended from arcades or canopies.

In addition to signs permitted by the sign code, any business may be allowed to have one additional sign, provided that it meets the following criteria:

- i. It is principally a 2-dimensional or 3-dimensional graphic symbol denoting what is offered by the business,
- ii. It is oriented to pedestrians, rather than to people in vehicles,
- iii. It is no more than 8 square feet in area per side.

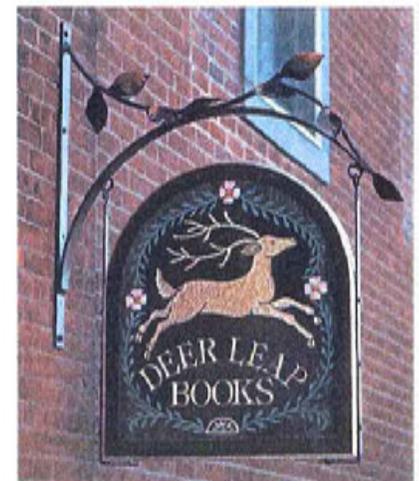
Consideration of who is viewing the sign and from where is important in sign design. Some retail spaces may be predominantly viewed from the pedestrian experience. In these cases, small scale identification signs placed at eye level can be very effective.



Signs Convey a Business's Unique Qualities

All signs whether lit or unlit, should utilize a contrasting background for legibility. Due to potential for glare, backlit signs are discouraged. Projecting signs supported by ornamental brackets and oriented to pedestrians are strongly encouraged. These must maintain an 8' clear height above the walkway. If located in service areas, projecting signs must be located out of the path of delivery trucks. The use of handcrafted metal sign brackets is encouraged.

Other Business and Residential Signage– For offices and for multifamily



Handcrafted Metal Sign Bracket

residential buildings, an emergency contact sign will be required near the main street entry of each building or store front. This sign will include emergency contact information and hours of operations. This sign to be no more than 1 square foot.

Directory signs will be needed in mixed use, office, and large multifamily residential buildings. Directory signs must be located inside the building's main entrance.

Boxes for display of menus, real estate listings, and other types of information display, shall be no greater than six square feet and no part is to extend above 6' - 0" from average grade.

Murals and super graphics are not generally allowed, but may be considered in special circumstances.

**Temporary/ Special Event Signs**– Signs that are temporary and for special events include signs for holidays, festivals, special events, political signs, etc. No temporary signs may be attached within 10' - 0" of an active storefront or main entry to a residential or office building unless it is for retail merchandise sales designed as a part of the overall window display.



Murals and Super Graphics

Temporary marketing signs and banners for the purpose of selling, leasing or renting residential, retail or commercial offices are allowed.

Temporary tenant identification signs are allowed during construction and up to 90 days after opening of the store.

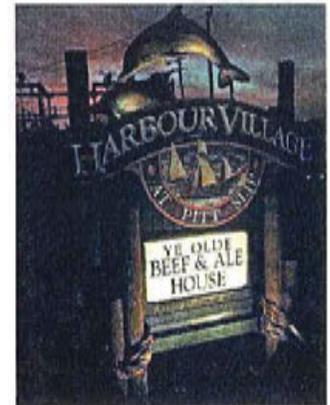
The following will not be allowed: pole signs; flashing, digital, or electronic message signs; and billboards.

The following will not be allowed: pole signs; flashing, digital, or electronic message signs; and billboards.

**Landscape Signs** – Monumental and other landscape signs are discouraged at Port Gardner Wharf. Each application will be reviewed on a case by case basis.

### Sign Sizes –

**Projecting and Hanging Signs** - Signs that are projecting or hanging from buildings or in arcades shall be allowed 2 square foot of sign face for each 10 feet on lineal front of the business, with a maximum of 10 square feet of sign face seen from either of two directions. Businesses having a frontage of 20' or less may use a maximum of 6 square feet.



Landscape Signs

**Wall Signs** – Wall signs on buildings or in an arcade shall be allowed 1 square foot of sign face for each 10 feet of lineal frontage of the business, with a maximum area of 8 square feet of sign face. Businesses having a frontage of 20' or less may use a maximum of 4 square feet. Wall signs for individual business with their own exterior public entrances must locate wall signs near the entrance.



Wall Sign

**Window, Door, and Awning**

**Signs** – Painted windows, doors, and awnings with permanent lettering and graphics are allowed for signage and may be used in lieu of sign boards. Size should be compatible with the architectural character, and not exceed 8 square feet.



## Chapter 3A: Streetscapes & The Pedestrian Network



### Section A: Streetscapes & The Pedestrian Network

#### A.1 Introduction

#### A.2 Pedestrian and Vehicular Circulation

- Street Network Plan
- Streetscape Layout

#### A.3 Streetscape & Pedestrian Network Standards

- Sidewalks and Walkways
- Intersections
- Traffic Circles
- Crosswalks
- Curb Ramps and Curbs
- On Street Parking and Transit Connections
- Streetscape Plants and Planting
- Illumination
- Streetscape Furnishings
- Fences and Railings
- Walls
- Street and Sidewalk Utilities

### Section B: Plazas & Open Space

### Section C: The Water's Edge

## Introduction

Port Gardner Wharf will be a pedestrian oriented, mixed use village with extensive public access and open space. Streets will provide access and circulation for vehicles, buses, bikes, and pedestrians, and form the matrix of internal linkages between retail, office, and residential uses, as well as marina areas and public spaces. The streets of Port Gardner Wharf are one of the most important building blocks in creating a rich and vital pedestrian environment.



Port Gardner Wharf will be a Pedestrian Environment

Streets and the streetscape for Port Gardner Wharf will:

- Establish the Backbone of the Pedestrian Network:** Sidewalks and walkways associated with streets will be the dominant means of access between the various districts, public spaces, and uses. Visitors and residents alike will come to Port Gardner Wharf via bus, boat, bike or vehicle, but once there, they will circulate using a well integrated and clearly established pedestrian network.



Street with Pedestrian Character



Street Market

- Provide Key Circulation Linkages:** The pedestrian system will provide ample access to all areas. A hierarchy of the circulation system is established to accommodate anticipated volumes for key linkages and destinations.
- Expand and Enhance Public Open Space:** With the Esplanade, the Amphitheater, marinas, and nearby park, Port Gardner Wharf will be a destination for public access to the wharf's shorelines. The pedestrian zone of the streets will expand these amenities by creating seamless connections and extensions. In addition, some street areas can be closed to vehicles for special events such as festivals or the farmers' market.
- Create the Port Gardner Identity:** The community will be experienced from the streets, mews, walkways and sidewalks. Urban Design of the streetscape is an essential part of the palette in creating the character of Port Gardner Wharf.
- Develop Livable Urban Space:** The circulation network will help to create a livable urban space where the focus is making a variety of intimate places.
- Minimize Impervious Surfaces:** Streets will have minimal lane sizes, but have ample street trees and planting areas to decrease impervious surfaces. Minimizing impervious surfaces is better for the environment and requires less water quality treatment.



## Street Master Plan

Every street will have its own character, but together they create a unified network. Within that network, streets will vary in size and function. Streets at the Port Gardner Wharf will fall into a typological hierarchy based on use, traffic and pedestrian volumes, location, linkages, and character of the neighborhood.

- **Boulevard:** The boulevard is a four lane road (an 11' and 12' travel lane in each direction) with a center median. 8' parallel parking or a 10' transit layover space shall be provided on each side along with 10' sidewalks.
- **Commercial Street:** These local access streets include two 12' travel lanes (one in each direction or

two one way) and 8' parallel parking on each side. Sidewalk widths will vary from 9' to 12' depending on street uses.

- **Neighborhood Street:** A residential street with two 11' travel lanes and 8' parallel parking on each side. Sidewalk widths will be 9' or 10' wide.
- **Alley's and Service lanes:** Service lanes will be 20' and alleys 24'. Both shall meet all fire access requirements.
- **Mews:** Pedestrian oriented alleys will be 12' to 20' wide.
- **Parking Aisles:** 24' wide two-way aisle with angled parking, and 16' wide one way with angled parking.

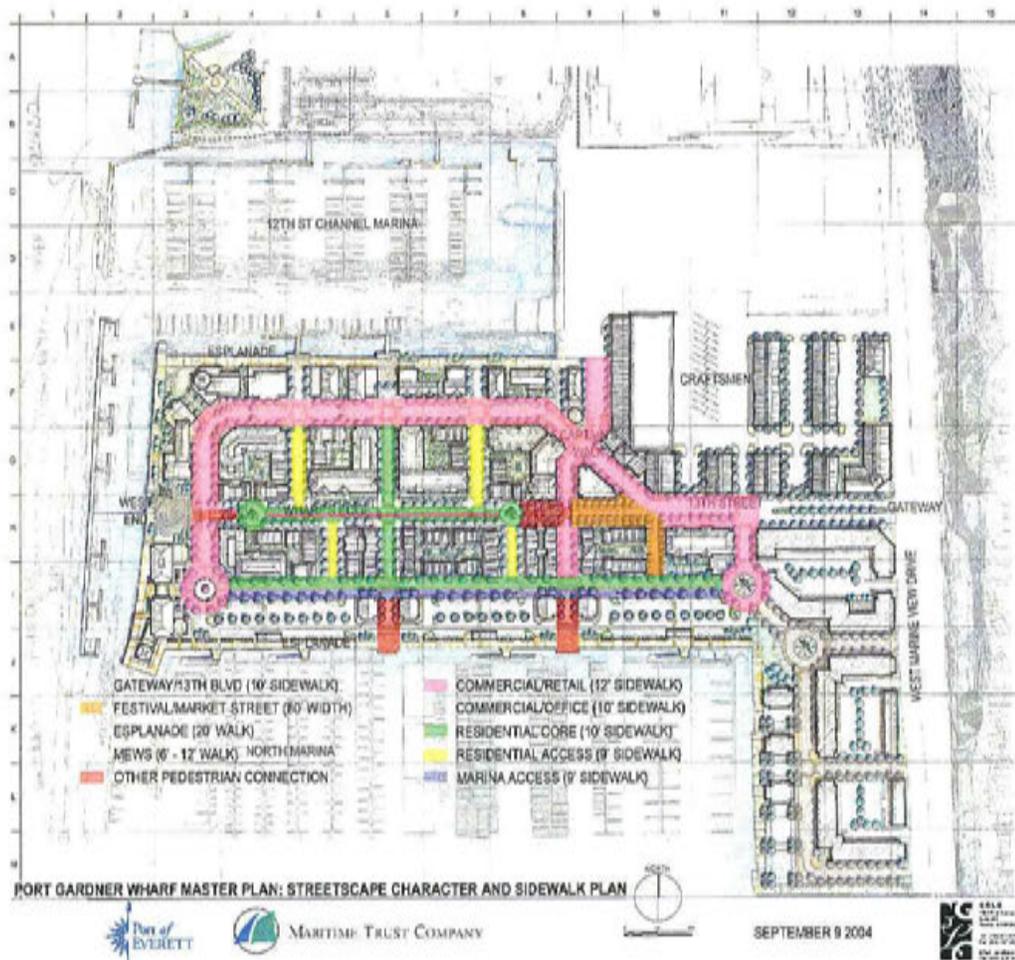


## Streetscape Character Plan

Within the hierarchy of streets, there are variations and specificity that establish the unique character of streets and pedestrian areas. Variation is envisioned to enhance the character of specific districts at the Port Gardner Wharf. The Streetscape Character Plan identifies some of the recommendations for specific treatments envisioned for key streets, sidewalks, walkways, and their locations.

Within each district, streets will take on an identity that reinforces that district's distinctive character. These identities will be established through urban design and streetscape elements covered in the next section entitled- Standards.

- West Marine View Drive: Frontage Improvements and center island median improvements to West Marine View Drive will give a public image of Port Gardner Wharf to the surrounding community.



Frontage improvements will include the addition of 12' sidewalks, street trees, and a planting strip. Planted islands with large street trees should be included in center lane medians of West Marine View Drive where turning pocket cue lanes are not required. This street is also designated as a bike route and appropriate improvements should be included per City requirements.

- **13th Street Boulevard Gateway Entry:** As the main entrance into Port Gardner Wharf, the 13th Street Boulevard gateway and collector arterial street will be a significant statement that establishes the identity and character for the community. In addition to special paving, lighting and planting in the median and street edges, an entry feature may be included.



Boulevard

- **Captain's Walk and The Retail Core Street:** This is the vibrant main retail street running through the site with major retail on 12th at the north edge of the wharf and along the Captain's Walk at 2nd Avenue, the east/ west connection between the 14th street Marina and Craftsman District. It will be a rich pedestrian corridor with active retail uses and enticing character. This central area is a pedestrian focused corridor linking associated commercial



Retail Street

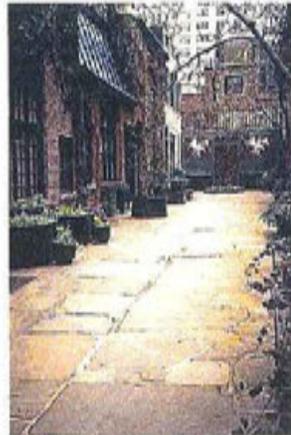
retail development across the Wharf. As a predominantly pedestrian zone, the street will be designed to maximize streetscape amenities. Vehicular traffic on these streets will be slowed.

- **Festival/ Market Street:** A Market/ Festival Street is aligned down the 13th Street corridor is also part of the Captain's Walk District. A unique street configuration and character is called for in the design of this street designated for festivals and markets. The street will serve as a retail area during normal times, but during Market or Festival Events, the area will be a pedestrian access only plaza. A special surface treatment and other streetscape elements will demarcate its uniqueness.
- **Craftsman District Streets:** Craftsman District includes all areas north of 13th Street east of 2nd Avenue from the water to West Marine View Drive. It has a secondary access at or in proximity to 12th Street. Since the Craftsman District focus is on marine services, special consideration is needed for requirements of the moving and storage of boats. The working waterfront character will be reflected in both the configuration and character of these streets. Other streets in the Craftsman District will primarily access existing buildings, yards, and future development. These streets will need to balance pedestrian and vehicular circulation and parking with truck access and boat yard activities. This will be done by defining areas and controlling potential conflicts through controlled access.
- **Major Neighborhood Streets:** 4th Avenue will be the main access into the residential area. Although predominantly residential, there may be some retail and office on the ground floor at corners of this street. Because this is a residential through street and will service larger multifamily residences and some commercial, a 10' sidewalk is necessary.



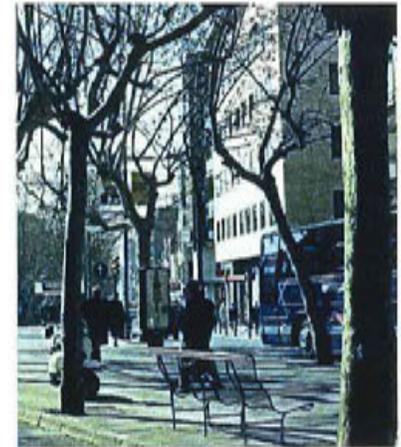
Residential Street

- **Minor Neighborhood Streets:** Internally to the residential area, minor access roads at 3rd Avenue and 5th Avenue will feed the residential building parking garages, provide on-street parking, and create a rich pedestrian environment. These residential streets will service residences with a 9' sidewalk and planting areas. The sidewalk will connect to town-house stoops where possible. Residential access may also be achieved with interesting entry courts.



Residential Entry Court

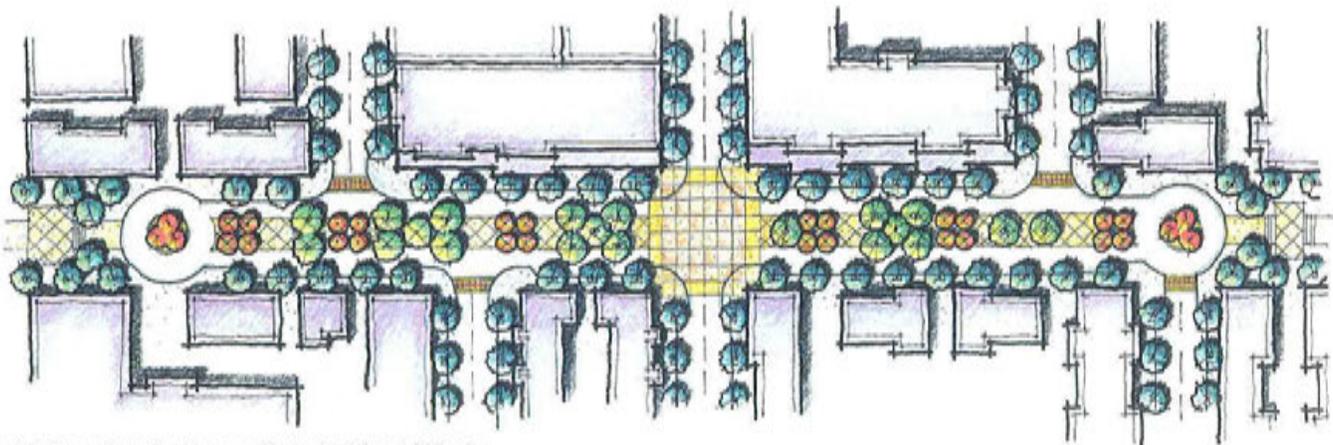
- **13th Street 'Ramblas':** As the main pedestrian spine and view corridor at Port Gardner Wharf, 13th Street through the residential core will create a significant public amenity. A wide median area called the 'Ramblas' creates character and passive recreation space for the residential community. The Ramblas is anchored on the east and west ends with cul de sacs that transition into pedestrian walkways tying to the Amphitheater on the West and the Market Plaza to the East. Special paving, lighting and planting will define a sequence of plaza spaces.
- **Gateway District Streets-** The Gateway District at the eastern portion of the property adjacent to West Marine View Drive from 13th Street to the south



An Urban Ramblas

property line will be a commercial office area. It contains a secondary access at or in proximity to 14th, 15th and potentially 16th. Ease of vehicular access will be balanced with a streetscape that will express the quality and character of the commercial office development. These streets will be a vibrant environment for workers.

- **Water's Edge Streets and Esplanade:** Water's Edge pedestrian zones such as the Esplanade require special consideration. These are covered in detail in Chapter 3C The Water's Edge. Streets in the Water's Edge District include the access streets to the Port Gardner Wharf redeveloped Marinas and parking. The main access street is 14th Street which provides access to the 14th Street Marina parking and is a seam between the water's edge and the upland residential area. As such, pedestrian access to the



13th Street Ramblas Concept in the Residential District



marina must be clearly marked with special paving and signage to ensure safe pedestrian transitions from the upland development.

- **West End Street:** The West End District defines the western end of the Wharf along the Snohomish River Channel. Access to the future hotel, restaurants, and Amphitheater will be from 6th Avenue which defines the eastern edge of this district. With the exception of parking and service vehicle access, this district will be focused on pedestrians. The crossing at the Amphitheater must connect seamlessly with the Amphitheater and provide easy access to the Esplanade.
- **Alleys, Service Lanes, and Mews:** These smaller streets create a finer grain network of access. Alleys blend utilitarian function with amenity. Some may be more utilitarian, but others will be focused more toward special consideration of pedestrians. Service lanes are short one way access points to building service areas that will be screened or treated to be visually benign. Narrow pedestrian focused alleys called mews will be designed with special paving and streetscape amenities to create interesting connective environments within Port Gardner Wharf.



Mews Create Interesting Spaces

### Sidewalks and Walkways

**Design Intent** – Sidewalks and walkways will be designed to create a pedestrian friendly environment with clear, safe, and aesthetically inviting sidewalks and walkways. Pedestrian sidewalks and walkways will include unobstructed durable surfaces. Sidewalks run along streets while walkways include all other access ways and connective paths associated with public amenities such as the Amphitheater and Water’s Edge Esplanade. Design must reflect the character of the Port Gardner Wharf urban waterfront environment through strong rectilinear planes with repetitive scoring patterns. The repetitive pattern will provide consistency and unity and form a simple background structure or framework on which the rich palette of other streetscape elements can be offset.

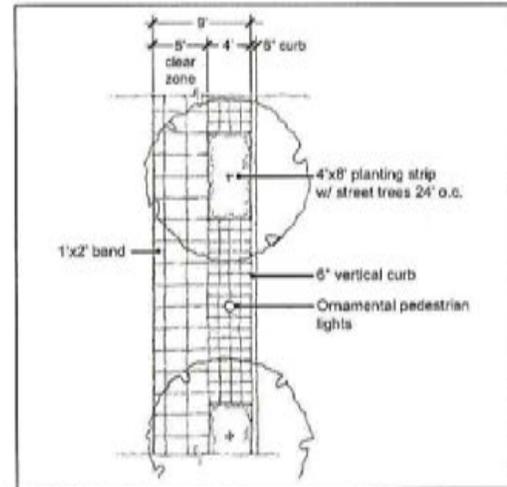
This consistent treatment will be set between special nodes that demarcate areas of special interest. These areas called ‘Access Nodes’ are lateral walkways that tie into various public plazas and open spaces. They provide pedestrian connections that will interrupt the rectilinear sidewalk and walkway pattern with special materials, colors, and patterns. The area on the edges of sidewalks and walkways provide area for planters, seating, low walls, and connections to plazas, fountains, and other public or semi-public amenities.



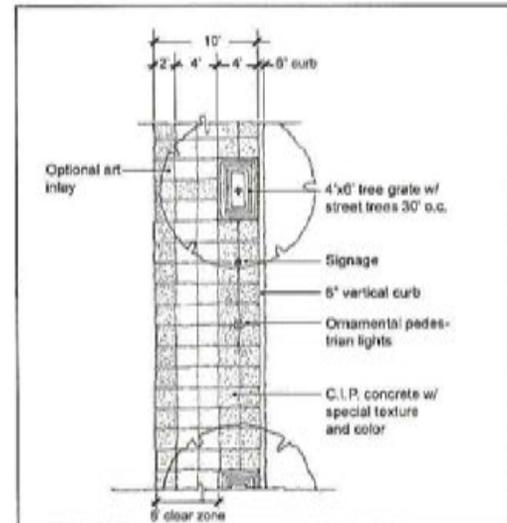
Sidewalk with Bench



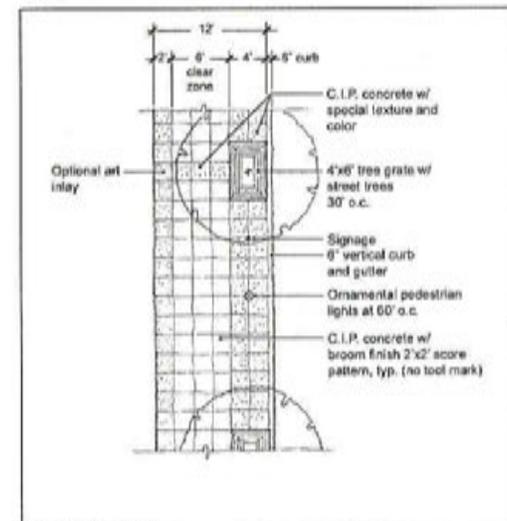
Access Node



Type 1 Sidewalk



Type 2 Sidewalk



Type 3 Sidewalk

**Dimensions** – Sidewalks and walkways will have a minimum 5' clear width Travel Zone. This width shall be continuous and unobstructed. As described in the streetscape layout section, several configurations of sidewalks and walkway will be included. Measuring from the inside of curb, these include:

- Type 1: 9' wide with 4' wide planter areas with street trees 24' O.C. Permeable pavers may be used in planter strip.
- Type 2: 10' wide with 4' wide tree grates or planter strip with street trees 30' O.C.
- Type 3: 12' wide with 4' wide tree grates widths or planter strips on the curb side. Street trees should be 30' O.C.
- Type 4: 14' wide total with a 10' sidewalk and a 4' wide planter strip which will include street trees 30' O.C.
- Type 5: 6' wide (min.) walkways with planting strips on one or both sides.

### **Accessibility**

– Pedestrian sidewalks and walkways shall comply with Chapter 11 of the ADA Handbook and all other accessible requirements and guidelines of the Access Board, State of Washington, and City of Everett.



Sidewalk with Planter Areas

### **Specific Sidewalk and Walkway Treatments –**

- West Marine View Drive: Frontage improvements will use the Type 4 sidewalk.
- 13th Street Boulevard Gateway Entry: The 13th Street Boulevard will use the Type 2 sidewalk. In areas near intersections parallel parking or transit

layover areas, bulb out space creates 8' to 10' of additional planter strip.

- Captain's Walk and The Retail Core Street: As the main retail streets running on 12th and 2nd Avenue, a Type 3 sidewalk will be needed. This will include a 3' wide area for sidewalk cafe table and shop spill out opportunities. For specialty shops and restaurants desiring more area, building design may create additional sidewalk frontage width with building setback and modulation.
- Craftsman District Streets: Craftsman District focus is on marine services. This area calls for the minimum Type 1 sidewalk in the few areas where a sidewalk may be needed. Some entry areas and retail frontage may increase the width as needed up to 10'.
- Festival/ Market Street: This unique street configuration is a pedestrian focused plaza with Type 2 sidewalks flanking a special walkable surface treatment. Design should explore combining the sidewalks and plaza/ street area.
- Major Neighborhood Streets: 4th Avenue will have Type 2 sidewalks for servicing the predominantly residential street that may include some retail and office on the ground floor at corners.
- Minor Neighborhood Streets: These residential access streets at 3rd Avenue and 5th Avenue will use the Type 1 sidewalks.
- 13th Street 'Ramblas': Sidewalks on both the north and south across from the 'Ramblas' median will be Type 2.



A 'Ramblas' Treatment

- **Water's Edge Streets and Esplanade:** Water's Edge Esplanade and walkways are covered in Chapter 3C The Water's Edge. The main access street to the 14th Street Marina is 14th Street which will have a Type 1 sidewalk on the south side and a Type 2 sidewalk on the north side.
- **Gateway District Streets:** The Gateway District commercial office development will have Type 2 sidewalks.
- **West End Street:** 6th Avenue along the West End District will have Type 2 sidewalks.
- **Alleys, Service Lanes, and Mews:** These will have a consistent treatment throughout and not be divided by curbs and sidewalks.



Permeable Stone Pavers

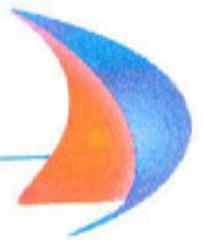


Mew - a Pedestrian Alley

Chapter III: The Public Realm

# Chapter 3B: Plazas & Open Space

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Section A: Streetscape & the Pedestrian Network

Section B: Plazas & Open Space

B.1 Introduction

B.2 Open Space

- Open Space Plan
- District Open Space Variations

B.3 Plazas & Open Space Standards

- Hardscape Surfaces
- Structures
- Planting
- Illumination
- Furnishings
- Fences and Railings
- Walls
- Utilities

Section C: The Water's Edge

Port Gardner Wharf will be a pedestrian oriented, mixed use development with extensive public plazas and open space. Plazas, courtyards, pocket parks, view point areas, the Amphitheater, the Esplanade, and other open space areas provide a connection to the environment creating a rich variety of exterior public spatial experiences for visitors, residents, and workers at Port Gardner Wharf.

Plazas and Open Space for Port Gardner Wharf will also:

- **Create the Heart of the Pedestrian Environment-** Just as sidewalks form the backbone of the pedestrian network, plazas and open space create the heart of the community. Plazas and other open spaces are integral to the overall site. Visitors and residents alike will come to Port Gardner Wharf because of the unique urban marine environment experienced through plazas and other open space.
- **Expand and Enhance Public Open Space-** With the Esplanade areas, the Amphitheater, marinas area plazas, and nearby park, Port Gardner Wharf will be a destination for public access to the shoreline and other public areas. These spaces shall be placed near each of the water edges of the site, and in areas with views to the water. These public spaces will be created in accordance to the development goals set forth between MTC, the Port of Everett, and the City of Everett. Total space provided in each plaza area shall cover an area approximate to that shown in the agreed upon preferred site master plan.
- **Create the Port Gardner Identity-** The public realm of the community will be experienced through plazas and open space. Urban Design of these areas is an essential part of the palette in creating the character of Port Gardner Wharf.
- **Develop Livable Urban Space-** Plazas and open space will help to create a livable urban space where the focus is making a place for the

enjoyment and health of residents, visitors, and workers.

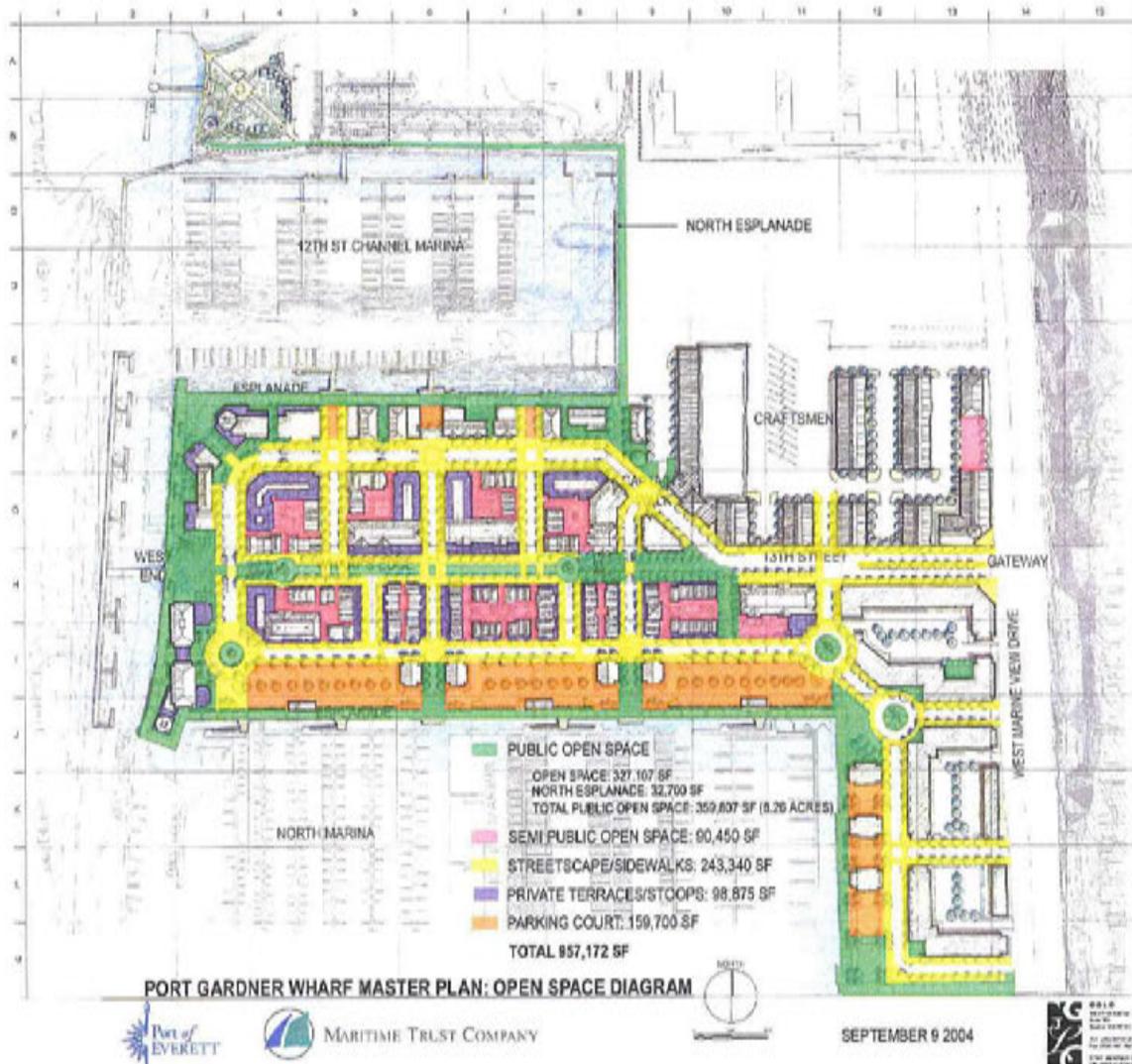
- **Make Some Spaces Interactive-** Some plazas and surrounding building facades shall be "interactive" meaning that a visual relationship is established between outdoor and indoor spaces and that there is transparency to the façades.
- **Promote Sustainable and Environmentally Friendly Practices-** Plazas and open spaces will be designed to improve the health and welfare of the residents and Everett community. By creating plazas and open space, especially ones with street trees and planting, impervious surfaces can be minimized. Minimizing impervious surfaces requires less water quality treatment which will be good for both the environment and for infrastructure costs.
- **Provide Key Pedestrian Nodes-** While the pedestrian system will provide ample access to all areas, plazas and open space areas form destination nodes of interest and activity. Size and use of plazas and open space will create a variety of pedestrian nodes.



Plazas Bring the Community Together

## Open Space Plan

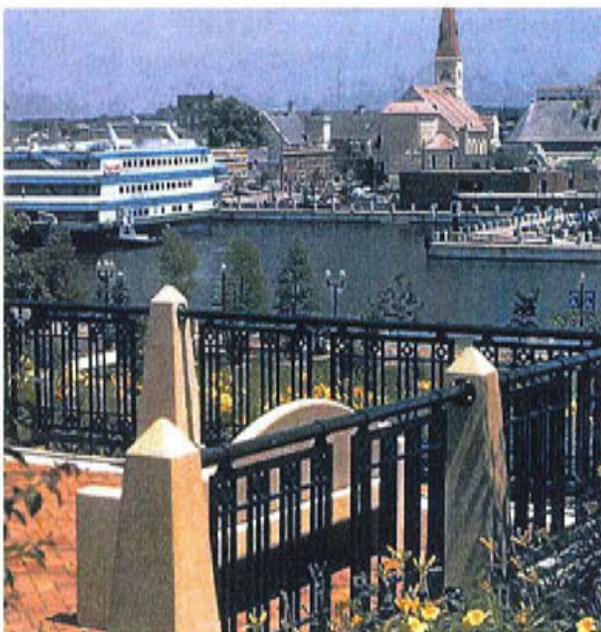
Every plaza and open space area has its own character, but together they create a unified network of open space nodes. Within that network, open spaces will vary in size and level of activity. Open space at the Port Gardner Wharf will fall into a general hierarchy based on use, anticipated amount of activity, location, views, and linkages.



- **Amphitheater:** Envisioned as a major public space along the waterfront at the Port Gardner Wharf, the Amphitheater will sit at the west end of the Water's Edge at the terminus of the 13th Street view corridor. The Amphitheater will include provisions for seating, a stage area and provision for power supplies and lighting.
- **Gathering Place Plaza:** These public plazas are to be the most active gathering places. They will serve to establish connections between buildings and other areas.



A Gathering Place Plaza



A View Point

- **View Points:** View points will be smaller scale plaza areas oriented toward views and designed to focus and enhance that passive use.



Residential Courtyard

- **Pocket Park/ Plaza:** These public parks and plazas are to be smaller scale public amenities that will serve residents and visitors.
- **Residential Courtyard:** Residential Courtyards provide semi-public open space meaning the space will be used primarily by residents, but neighbors and visitors will not be excluded. These courtyards at the larger multifamily residences may vary in size, but they will be significant amenities to the residential area. Access from residential units and streets are important in the way the courtyard is defined.
- **Esplanade Transition and Marina Access Nodes:** Plaza Nodes along the Esplanade and pedestrian access areas to the marinas are the major Water's Edge pedestrian zones. These Transition Nodes are plaza areas that connect the Esplanade to the upland retail and residential areas of Port Gardner Wharf. They must be carefully designed to ensure safe pedestrian transitions from the upland development, across parking areas, and to the marinas. These and other Water's Edge pedestrian zones are covered in detail under Section 3C The Water's Edge.

## Plazas & Open Space

- **Pedestrian Alleys and Mews:** Pedestrian focused alleys called mews share characteristics with plazas and open space. Although design guidance for these is presented in Section 3A Streetscape & The Pedestrian Network, some design considerations overlap in the connection between pedestrian alleys and mews and plaza nodes.

### District Open Spaces

Within each district, open space design will take on an identity that reinforces that district's distinctive character. These identities, described below, will be established through urban design elements covered in the Plaza and Open Space Standards.

- **Gateway District** - This area sits along the south eastern portion of Port Gardner Wharf and runs adjacent to West Marine View Drive from 13th Street to



13th Street Corridor

the south property line. The Gateway District's commercial office uses will include gathering point and pocket plazas that will serve the work force of this area of development. A small park will also be developed at the Esplanade where 14th

Street enters through the Gateway District. Design of these spaces will be less traditional with the office professionalism expressed through repetitive lines, forms, and geometries.

- **Craftsmen District** - Craftsman District's Marine Commercial, Retail, and Light Industrial areas sit north of 13th Street from the water to West Marine View Drive. The working waterfront quality will be expressed through durable maritime forms and "industrial" details. Small plazas will create lunch and break areas for craftsman district workers and visitors.

- **Captain's Walk/ 12th Street Residential** - As retail heart of Port Gardner Wharf, this area will be well-detailed, unique, expressive, and spectacular. Materials, forms, and spatial layout of plazas should reinforce the maritime heritage. In this predominantly pedestrian zone, the street will be the central focus of energy, but connectivity to the Market/ Festival Plaza and other plaza areas will be carefully integrated. They will be designed to maximize pedestrian amenity and character. Because the Market/ Festival space is part streetscape and part plaza, its character is further defined in Section 3A Streetscapes & The Pedestrian Network. During events, the area will be closed to traffic and be a pedestrian-only plaza.



A Market Plaza

- **Residential Neighborhood** - The Residential District takes up the large central western portion of the Wharf inside the 200 foot shoreline management zone. This area will be predominantly residential with some retail and possibly office on the ground floor near main intersections. Open space will help to create a strong community identity in this district. Courtyards, pocket parks, mews, and the 'Ramblas' are open space amenities that will be part of the residential neighborhood character. These areas will express a classic traditionalism through rich decorative detailing and forms. The 13th Street 'Ramblas' will act as the main pedestrian spine and

a significant public amenity through the residential core. The wide median area will create passive recreation space for the residential community and will greatly enhance the district's character.

- **West End** - The West End District defines the western end of the wharf along the Snohomish River channel with restaurant, hospitality, and Amphitheater areas. The Amphitheater is a major public amenity space. Design elements in the Amphitheater and other public spaces in the West End District will be less traditionally decorative with strong simple forms. These spaces will meld rather than distract from water view and from the unique character of the restaurants and hotel. Spaces will connect seamlessly with the Amphitheater and provide easy access to the Esplanade.
- **Water's Edge (Esplanade)** - The Water's Edge District includes the redeveloped Marinas and bulk head improvements wrapping the entire Port Gardner Wharf. Full public access will be provided to the great pedestrian amenity of the Esplanade. Plazas and view points will enhance upland connections to the Water's Edge. As described in Chapter 3C The Water's Edge, design will reflect the maritime character and aesthetic with boat-like expressive forms and details.



Plaza Surfaces Can Define the Space

### Plaza & Open Space Hardscape Surfaces

**Design Intent** - Plaza surfaces and associated connective paths will be designed to be both safe and aesthetically inviting. Hardscape material surfaces will be high quality, durable, and designed to provide safely unobstructed routes of travel and play. Treatments of surfaces through colors and patterns will create environments that are inviting and help create a coherent



Plaza Surface Patterns

sense of place. Design must reflect the character of the Port Gardner Wharf urban maritime waterfront environment and, at the same time, create unique nodes that demarcate District qualities. Surface patterns will use repetitive elements to provide consistency and unity and form a simple background structure or



Brick Pavers

# Plazas & Open Space

framework on which plaza furnishings and other elements can be offset. Plaza edges provide areas for planters, seating, low walls, and connections to other public or semi-public amenities.

Dimensions - Plazas will be of multiple sizes and shapes to fit the particular constructs of their context. Areas defined for pedestrian movement will have a minimum 6' clear width travel zone. This width shall be continuous and unobstructed.



Sidewalk Cafe' Umbrellas

## Plaza and Open Space Structures

Trellis and Arbor Structures - Trellises and arbors will define spaces and provide semi-enclosed areas at the edges of plazas, courtyards, and pocket parks. They provide areas for privacy and shade, and structure for vines to grow on. Trellises and arbors should be integrated into the design of plazas and open spaces. Materials must be stainless, galvanized, or painted steel, or they may be wood with weather protected stain or paint. These structures, if wood, should be natural in color or white. Steel, if not galvanized or stainless, shall be painted a dark color or white. Structures must also meet all applicable City codes.

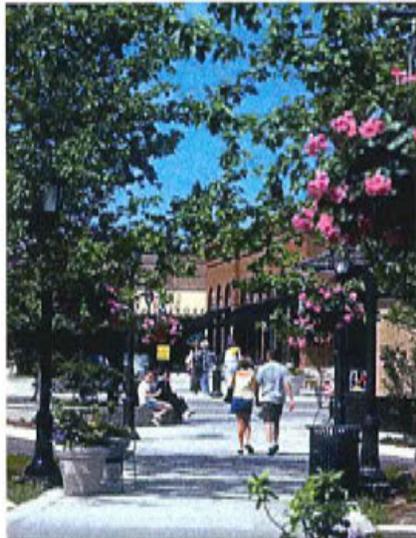


Arbor Structure

Umbrellas - The use of umbrellas is encouraged in plazas and other open spaces, especially those in proximity to cafés and restaurants.

## Plaza & Open Space Planting

Design Intent – Plaza and open space planting at Part Gardner Wharf will be a major design element for enhancing character and the quality of place. Plants will define the space and add scale, visual interest, and seasonal change. Layout and plant palettes selection will reinforce and define the character of each plaza and tie in with the district identity. Plants shall be appropriate to this urban marine environment and emphasize the unique qualities of their context. Planting shall be selected and placed to enhance rather than obstruct views. Selections shall require low-water use, and grouped in associations with similar microclimate needs for sun, soil and water requirements. All planting shall be irrigated to ensure health and survival.



Plants Enhance the Character and Quality of Place

Recommended Street Trees – Deciduous street trees shall be used in plazas and open space to reinforce pedestrian connections, define edges, and provide shade



Trees Reinforce Pedestrian Connections through Open Space

for seating areas. Trees in view lines of residential areas should be columnar, or branched clear to 8' high and have an open canopy. Trees will also play a major role in establishing identity of the various open space areas.

### Trees for Specific Plaza Types:

- **Amphitheater:** The Amphitheater site at the west end of the water's edge at the terminus of the 13th Street view corridor calls for careful placement of small and open canopy trees to minimize view impacts from the upland residential units to the water. Amphitheater audience area may include small scale ornamental trees set behind the seating. The stage area will be kept open for clear viewing from the seating area. Trees may also be used to buffer restrooms and service areas from the Amphitheater.
- **Gathering Place Plaza:** Trees in these active public plazas will serve to emphasize connections and define these spaces as major nodes. Trees at these plazas should create a unique sense of place reflective of the district or area design character. Any tree on the list may be appropriate for Gathering Place Plazas.
- **View Point Plaza:** As small scale plaza areas oriented toward views, trees, if used, will be small, decorative, and open canopied. They will focus and enhance rather than obstruct views to the water.
- **Pocket Park/ Plaza:** Serving primarily residents, these public areas provide a respite for the urban living and should use a variety of medium and small scale trees to shade, define spaces, and add color, texture, and seasonal change.
- **Residential Courtyard:** Residential courtyards are significant amenities to the residential develop-



Plaza Trees Create Shade

ment. Because courtyards will be on garage roofs, medium and small scale trees must be selected that do well in boxes or contained planters. Another consideration is preservation of views - trees must be open canopied.

- Festival/ Market Street Plaza: Trees along this unique street configuration aligned down the 13th Street corridor create a space that is part streetscape and part plaza. Large-sized, open canopy trees in grates will flank both sides and create the market identity and frame the 13th Street view corridor. Streetscape trees appropriate for the market plaza are covered in Section 3A Streetscape & The Pedestrian Network.
- Esplanade Transition and Marina Access Nodes: Trees appropriate for these plaza and open space areas at the marina are covered in Section 3C The Water's Edge.
- Alleys and Mews: Trees that are appropriate for mews are covered in Section 3A Streetscape & The Pedestrian Network.

**Understory Planting** – Shrubs, hedges, grasses, ground-covers, and perennials shall be used in plaza and open space planting areas to provide buffers, define spaces, and add color, texture, scent, and seasonal change. The plant palette should be refined and elegant and reflect the character of the district area through form and texture. Layered planting should provide four season interest. Lawn may be used in selected Pocket Park and Courtyard Areas.



Hedges Define Space at this Pocket Park Garden

**Tree Grates** - Plaza trees will be set in planters or in tree pits with tree grates. Tree grate surfaces in walkway areas shall be ADA compliant. The size is to be a minimum of 4' x 4', but larger grates should be used for larger trees. Each plaza should use only one type of tree grate. Plaza designers should explore the possibility of a custom designed tree grate to help enhance the unique character of the plaza and character of the Port Gardner Wharf.



Planters, Pots and Boxes Create Edges

**Planters, Pots, and Boxes** - Planters may be a part of plazas, pocket parks, and other open spaces. Planters should be raised 12" to 24" with seat walls at the edges to protect plants from pedestrian short cut paths and trampling. Pots and planter boxes will be used primarily in the residential courtyard plazas where building maintenance can provide care. Plant pots and boxes must be at least 18" deep and have a minimum of a 18" diameter.

**Planting Integration with Art** - Art in plazas can use planting to complement and enhance the art with color, fragrance, visual texture, and seasonal change. Consideration should be given to using integrated art in plazas that may use plantings as part of the art installation.

## Plaza & Open Space Illumination

**Design Intent** – The illumination of plaza and open spaces will increase safety and security, and add a provocative evening character. Illumination can add richness to the Port Gardner’s unique urban maritime waterfront environment concept and character. The image and personality of the place will be expressed not only through fixture type and location, but also through elements such as the incorporation of specialty logos or images on the lights, poles or pole bases. Lighting types include:

- Area Lights for larger open spaces and plazas
- Pedestrian Lighting for circulation areas including bollard lights and lighting on steps
- Accent Lighting on special architectural and landscaping features
- Building Illumination as it affects the plazas
- Special Event and Decorative Lighting



Creative Plaza Lighting

**Dark Sky Requirement** – Fixtures for plazas and open space will be selected to minimize light pollution. The development agreement calls for a dark sky to be maintained for nearby residents. Dark sky criteria includes using lighting that is shielded from the night sky and adjacent residential properties through down casting, exterior shields, and/or fixture optics.

**Illumination Quality and Levels** – Light quality will have well balanced color produced by metal halide, halogen,

or incandescent bulbs. No high pressure sodium lights are allowed. Footcandle levels shall meet the IESNA recommended standards. These standards ensure that lighting levels function in a manner that provide safe and clear illumination to pedestrians, and that the entire development is properly lit to ensure a safe and secure neighborhood.

**Placement** – The placement of lighting fixtures in plazas and other open spaces should be discrete and unobtrusive so as to eliminate physical clutter and avoid glare into residences and commercial offices. Placement of lighting fixtures should also be done to facilitate maintenance such as re-lamping.

**Maintenance and Operation** – Lighting fixtures should be placed and designed to minimize maintenance and operation costs. This includes maximizing lamp life through the use of motion detectors, photo-voltage switches and timer switches. Where possible, lighting may serve more than one illumination function.

**Materials** – Exterior lighting will be constructed of materials durable in the marine climate. Fixtures should have vandalism protection where possible. Materials include stainless steel, heavy gauge aluminum, and metals with a powder coat finish.

**Finishes** – Finishes will be durable and low maintenance. Finishes shall be stainless, galvanized, or powder coated.

**Colors** – For exterior light fixtures that are not stainless or galvanized, finishes should be black, or use a dark color that fits into a color scheme that is appropriate to the District.

### Types of Exterior Lighting -

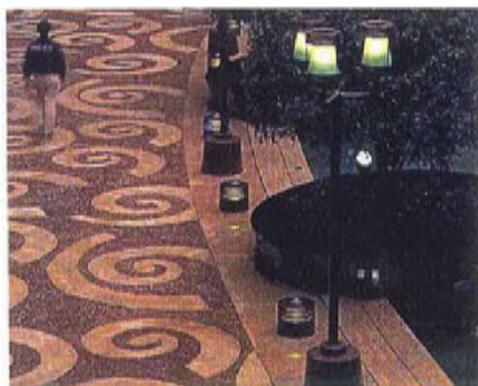
**Area Lights** - Lighting of large gathering plazas and the Amphitheater may use pole mounted fixtures with twin arms for an extended light on one arm and either flower baskets, banners, or theme medallions suspended from the other. Lighting shall mount on poles that are no higher than area second story residential window levels. Light must be focused downward and shielded from the night sky and from residences.

**Pedestrian Lighting** – For safety, security, and for aes-

## Plazas & Open Space

thetics, pedestrian lighting is an important aspect of plazas and open spaces. Pedestrian lighting will be achieved with wall mounted lights, illuminated bollards, footpath lights, or in some cases pole style lighting. Where possible, use either recessed, wall mounted, or free stand-

ing bollard light fixtures that keep the light low to illuminate the ground plane and seating surfaces. Inset stairway and stair step lights are required on all stairs.



Plaza Bollard and Pole Lights

- Pole style pedestrian lights are smaller than area lights and may be either fixtures on poles or illuminated poles. These lights are 10' maximum height will have lower illumination than area lights. Light must be focused downward and shielded from the night sky and from residences to minimize any potential impact on views. Poles may include arm extensions for planted baskets.
- Bollard lights will be used at pedestrian ways where plazas are adjacent to connecting roadways and parking, or they may be used for lighting the ground plane of plazas. They can enhance pedestrian safety, way finding, and ornamentation.
- Other pedestrian illumination for plazas and open space may include wall-mounted fixtures on buildings bordering plazas. These could double to illuminate flower beds/ planters or other features as well.



Bollard Light

**Accent Lighting** – Accent lighting will be used to emphasize or mark plaza special features such as fountains, walls, sculpture, planters, or trees. Fixtures shall



Built-in Accent Lighting

be inconspicuous and durable should be recessed in walls, ground planes, or steps. Accent lights may also serve multi-purposes such as the illumination of public signage.

**Building Illumination** – Building mounted lights will be used to light walkways, terraces, and plazas in appropriate areas. Building lighting shall be designed to cast downward and be shielded to minimize glare. Because building lights may be turned off, building lighting shall not be depended on exclusively for illuminating plaza walkways.

**Specialty Lighting** – Specialty lighting may include decorative lighting or event lighting for the Amphitheater and the festival market.

- Event Lighting – Events at the Amphitheater or festival market will require high safety standards. Lighting must meet all safety requirements for public gatherings. Temporary event lighting should be considered and designed for to be easily and effectively managed for event times. Design must also include a site sensitive integration of permanent lighting, considering the Amphitheater will serve most of the time as a public plaza.
- Decorative Lighting – Decorative lights may be for special effects or for celebrations. Seasonal celebrations include decorative lighting for holidays such as Christmas lights on trees. These should be planned for by providing access to safe exterior electrical power sources. Special effects lighting such as

neon, running lights, and other types of colored or decorative lighting is only allowed in plazas along the retail areas of the streetscape. It may also be appropriate for signage or décor in buildings or as part of art installations.



Temporary Event Lighting

#### Light Fixtures for Specific Plazas and Open Space Areas

- **Amphitheater:** Light fixtures at the Amphitheater will meet needs specific to both the design and function of the Amphitheater. Event times will vary greatly from most evenings creating the need for fixtures and controls to meet higher illumination standards for special event use and low levels for other times. The fixture types will also help establish the unique character and quality of place. Area Light Fixtures will cover main access, stage, and support facility areas. Pedestrian Light Fixtures will be used for the seating and walkways, as well as for the transition areas from the Amphitheater to the Esplanade. Accent Light Fixtures will help to define the seating during events and to provide accenting. Specialty Light Fixtures will include theatrical lighting appropriate for the exterior all weather conditions.
- **Gathering Place Plazas:** Main public plazas off of the major active streets and pedestrian corridors will require higher levels of illumination than other plazas. Light fixtures should have special characteristics such as custom metal emblems which will help define and brand the area. Area Light Fixtures will illuminate main access areas. Poles may include

planted baskets and brackets for banners. Pedestrian Light Fixtures will cover seating areas and walkways. Accent Light Fixtures will define special focal points and edges.

- **Pocket Park Plaza:** Light fixtures in these locations create a strong community identity within the residential area. Light should be focused on the park area and levels should meet minimum standards. Area Light Fixtures cover main access areas of the larger residential spaces. Poles may include planted baskets. Pedestrian Light Fixtures will illuminate seating areas and walkways. Accent Light Fixtures will define special focal points.
- **Residential Courtyard:** Lights at residential courtyards create a strong community identity within the residential area. Light must not intrude into residential units and will need to be focused on the courtyard area with minimum, but safe illumination levels. Fixture types will help tie the courtyards to the identity of the district. Area Light Fixtures will cover main access areas in some of the larger residential courtyards. Pole placement must work to preserve views. Poles with arms for planted baskets may be included. Pedestrian Light Fixtures will define seating areas and illuminate walkways. Accent Light Fixtures will define special focal points and to provide accenting.



Courtyard Accent Lighting

- **View Point Plaza:** Lighting of these small scale plaza areas shall illuminate the ground plane and define edges, but must not interfere with views. Pedes-

trian Light Fixtures should only illuminate seating areas and the ground plane. Accent Light Fixtures can be used to define edges.

- Festival and Market Plaza: This unique street/plaza configuration should be illuminated dominantly by the pole mounted street lights. These streetlights along the sidewalks will cast light both on the sidewalk and in the market street area. Light fixture types will help create the market identity and identity for the 13th Street view corridor. These are covered in Section 3A Streetscape & The Pedestrian Network.



Inset Accent Light

- Marina Access Nodes: Area lights for these pedestrian access areas to the marina are covered in Section 3C The Water's Edge.
- Alleys and Mews: Lighting for alleys and mews shall increase safety and security and enhance ambiance. Light must not intrude into residential units and will need to be focused on the alleys and mews with illumination levels meeting the minimum standards. Fixture types will help tie these pedestrian areas to the identity of the district. Area Light Fixtures will cover main access areas in some of the larger alleys. Pole placement must work to preserve views. Poles with arms for planted baskets may be included. Pedestrian Light Fixtures will illuminate walkways. Accent Light Fixtures will define special focal points and to provide accenting.

District Plaza and Open Space Light Fixtures – Some fixtures may be of the same family to provide a level of consistency, but their sub components vary to accentuate the differences of the districts.

- 12th Street and the 2nd Avenue Captain's Walk: Plazas in the main retail district require higher levels of illumination and special characteristics such as custom metal emblems and poles with brackets for planted hanging baskets and banners to add definition. Fixtures will be well detailed and evocative of

the maritime heritage. The market/ festival street configuration will use streetlights along the sidewalks that cast light both on the sidewalk and in the market street area.

- Craftsman District: The marine services district calls for minor plaza lighting predominantly for safety and security. Fixtures must relate to the marine services, working waterfront character with durable maritime forms and details.'
- Residential Area: Residential plazas, courtyards, and pocket parks will have fixtures that create a strong community identity within the residential area with traditional decorative detailing and forms.
- Gateway District Streets: This area will use courtyard fixtures that are less traditional and express professionalism through simpler forms, lines, and geometries.
- West End District: The Amphitheater will use a less traditional design palette with strong but simple forms that meld rather than distract from water view and the unique character of restaurants and hotel.
- Water's Edge: Plaza's and open spaces along the water will use fixtures that express the maritime quality with boat-like sleek forms and details. These materials are shown in the Water's Edge Section Chapter 3C.



Bollard Light

## Plaza & Open Space Furnishings

**Design Intent** – Exterior furnishings will provide public amenities that establish a high quality urban refuge in plazas and open space. Design will reflect the context of the area and help to establish the unique qualities of place within the larger Port Gardner Wharf context. These elements will be integrated into the overall design of each district. The amount of exterior furnishings should be minimized and where possible, necessary elements should have multiple uses; for example, retaining walls should be designed as seat walls.



Place Benches for Viewing and Catching Sun

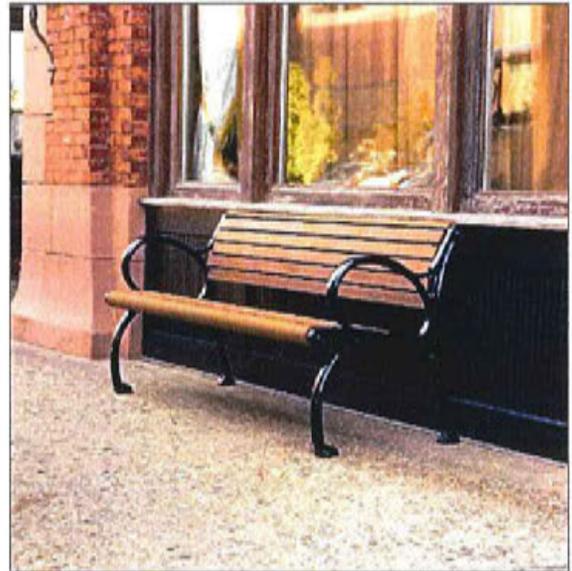
**Materials** – Exterior furnishings will be constructed of materials durable enough for urban use and the marine climate. This includes steel, heavy gauge aluminum, and masonry. In some cases, wood that is well treated to hold up in the marine environment may be appropriate.

**Finishes** – Finishes will be durable and low maintenance. Finishes for metals shall be stainless, galvanized, or powder coated. Finishes for masonry shall be smooth. Finishes for wood shall also be smooth, but must also include appropriate protective sealing and treatments for longevity.

**Colors** – For exterior furnishings that are not wood, stainless steel, or galvanized, finishes should be black, near-black blue-green.

### Types of Exterior Furnishings:

**Benches** – Benches will be integrated into plaza and other open space areas. They will be located in public areas that provide opportunities for views out to the water, people-watching, catching sun, waiting for others, and finding some semi-private space for reading and relaxation. When possible, bench design shall include integration into walls and planters. Free stand-



Bench

ing benches will be selected for durability and design. Benches must minimize water accumulation on the seating surface and deter abuse from skateboards or other vandalism. They also must relate to the aesthetic of the connecting street, district, or area.

**Trash Receptacles** – Trash cans shall be located at or near all active public gathering areas especially those located near concessions and cafes. Trash cans must be lidded and enclosed to keep out seagulls and rodents. They also must be durable, easy to maintain, and easy to use when changing bags.

**Bollards** – Bollards shall be used to protect pedestrians from vehicles in areas where pedestrians access public plazas. Bollards must be well proportioned and can be either permanent and removable. Permanent bollards can be used to define edges and entrances to pedestrian areas and control vehicular access. They may integrate illumination in pedestrian areas. Removable bollards are to be placed in areas where emergency or maintenance access may be required. Bollards will relate to the aesthetic of the district and may be custom integrated art, reused wharf materials such as steel piers or granite slabs.

**Bicycle amenities** – Bike racks may be placed out of pedestrian circulation ways on the edges of plazas or open space areas where bikers may need to park their bikes and wish to walk, shop, eat, or rest. Access to and

from the bike racks should be clear and unobstructed. Bike racks must be double-poled and wide for resting the entire bike against the rack rather than single-poled. They should relate to the aesthetic of the plaza in form and placement.

**Signage** – Plazas, in general will not include signage except interpretive signs or possibly an information kiosk. Interpretive signs should be limited in size to 24" x 32". Signs and information kiosks shall not block views or pedestrian flow. For more information, see the Signage Guidelines in Chapter 2.

**Special Event Banners** – For special festivals, markets, or events at the Amphitheater, small banners may be used on a temporary basis for a maximum of one month. Banners may be attached to one side of area lights. Banners must not contain private advertising.

## Plaza Fences, Hedges, and Railings

**Design Intent** – Fences, hedges, and railings at the edges of plazas, courtyards, and other open space will provide safety, security, and in some cases privacy. Design of fences and railings will be compatible with other elements through form, materials, and finishes. Design will reflect the aesthetic of the Port Gardner Wharf and will be informed by the district or area character.



Railings Provide Safety and Define Plaza Edges

**Materials** – Exterior fences and railings will be constructed of materials durable in the marine climate. This includes stainless steel, heavy gauge aluminum, and steel with the appropriate finish.

**Finishes** – Finishes will be durable and low maintenance such as stainless, galvanized, or powder coated.

**Colors** – For exterior fences and railings that are not stainless or galvanized, finishes should be black.

### Types of Fences and Railings:

- **Fences** – Fences will be used for screening, security and separation of space. For security areas, 5' to 8' tall fences will provide controlled access, but maintain visibility such that no one can hide behind any given area of the fence. For the screening of trash dumpsters and utility areas, a 6' high fence or hedge is required that will provide 100% screening when closed. In no case shall chain link fences be allowed except as a temporary solution for construction security.
- **Hand Railings** – Hand railings will be used for stairways, steep ramps, and other plaza areas where a rail will help assist in balancing as one transitions grade changes. Hand rails must meet all required codes and standards for accessibility.
- **Guardrail** – In areas where there is a steep grade drop-off or other potential safety hazard, guardrails will be used. Guardrails shall be meet local codes and standards.



Fence with Art and Visibility

## Plaza Walls

**Design Intent** – Walls provide edges, grade retention, and definition, but also will enhance the urban design character of Port Gardner Wharf plazas and open space. Where possible, walls shall be designed as 12” to 24” high seat walls to provide opportunities for public seating. Design will reflect the Port Gardner Wharf environment through recalling historic wharf and sea wall design with materials, stone or masonry unit size, and joinery. Rockeries are not appropriate upland of the shoreline.



Courtyard with Walls

**Materials** – Walls will be constructed of rock and masonry materials durable enough for the marine climate. This includes granite, brick, and concrete. Where possible, granite shall reflect historic type and coloring found in regional historic wharf conditions.

**Finishes** – Wall finishes for granite will expose the natural material, but should be textured to various degrees according to its application. Brick finishes shall be unpainted natural brick that has a historic look. Finishes for concrete walls should be smooth and durable or lightly imprinted with custom form liner patterns that tie to the district or area character. Concrete should have ‘V’ groove control joints or channels at frequent intervals. Sealants shall be required in areas with potential exposure to vandalism or staining. All finishes must be long-lasting and low maintenance.

**Colors** – Granite and brick shall be unpainted with natural color selected to enhance the historic character. For concrete walls, color should be integral or stained

with a permanent, non-fading penetrating stain finish. Stain color should adhere to the color palette of the surrounding context.



Granite Wall

## Plaza and Open Space Utilities

**Design Intent** – Utilities link basic systems and services throughout Port Gardner Wharf. These utilities will tie to and service plaza and open spaces. Although the design of utilities is often driven by utilitarian needs and constraints, their location and coordination with other elements can strengthen the overall urban design character and aesthetic of plazas and open spaces. Where ever possible, utilities will be consolidated and located below grade. If utilities must be above ground, they will be screened or concealed away from active public areas.



Coordinate Utilities Locations with Plaza Design

## Plazas & Open Space

Utility lids and other exposed elements that can not be concealed or screened must be designed or selected to work with the overall aesthetic of the plaza's district character.



Simple and Elegant Drainage

**Location** – Locating exposed utilities is critical for maintaining quality design treatments. All utilities that are not under grounded, concealed, or screened must be located in coordination with exposed elements. They will be aligned and integrated with the layout of other design elements.

**Materials** – Utility grates, covers and other exposed elements will be constructed of materials durable enough for the marine climate. This includes cast iron, brass, steel, heavy gauge aluminum, and masonry.

**Finishes** – Finishes will be durable and low maintenance. Finishes for metals other than brass or cast iron shall be stainless, galvanized, or powder coated. Finishes for masonry shall be nonporous. Finishes with protective sealing and treatments for longevity should also be considered.

**Colors** – For exterior utilities that are not cast iron, stainless, or galvanized, finishes shall be black or silver.

### Types of Utilities:

- **Utility Grates** – Grates such as trench drains and catch basin covers must be both functional and decorative. This is especially important in plaza and courtyard areas of high pedestrian use. Drain covers



Plaza Trench Drain

should be custom designed or decorative throughout the Port Gardner Wharf (such as Urban Accessories 'Nautilus'). These drains can emphasize the connection to the marine environment.

- **Utility Lids and Covers** – Cast iron and galvanized steel utility lids and covers are potential elements for integrated art and design. Designs should visually indicate the type of utility (electric, water, gas, etc.) and tie with the district's aesthetic. Concrete lids are to be placed in areas out of sight and out of travel ways.



Utility Lids

- **Utility Cabinets and Boxes** – Cabinets and boxes for utilities must be placed in areas out of sight and out of travel ways. They also should be finished in dark earth tone colors or materials that blend into the setting.
- **Utility Corridors and Access** – For utility banks and lines where future access may be needed, design of surface treatments should take this into account. Design can reflect the area and use materials that can be easily replaced or seamlessly patched.

Chapter III: The Public Realm

# Chapter 3C: The Water's Edge

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Section A: Streetscape & the Pedestrian Network

Section B: Plazas & Open Space

**Section C: The Water's Edge**

C1 Introduction

C2 Characteristics of the Waterfront

C3 Water's Edge Standards

- Esplanade and Other Pedestrian Walkways
- Marina Parking
- Water's Edge Plants and Planting
- Lighting
- Water's Edge Exterior Furnishings
- Fences and Railings
- Walls
- Utilities
- Vehicle and Dock Access Facilities



## Introduction

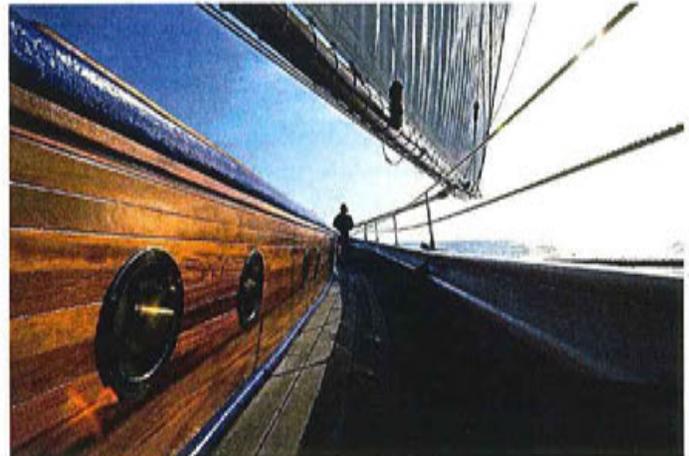
The Water's Edge is a continuous pedestrian way encompassing Port Gardner Wharf. The area includes the Esplanade as well as key connective spaces and passages. As an amenity for pedestrians and boaters, it provides public access along the shoreline, as well as access to the marinas and other site amenities. The Water's Edge Esplanade running along the shoreline will connect the 14th Street Marina (to the south) to the proposed 12th Street Marina and 10th Street Marine Park, both to the north. The Esplanade will frame the community of residences, retail spaces, restaurants, hotels, offices and marine related facilities that make up Port Gardner Wharf.

The Port of Everett is leading the design and construction of projects which make up much of the Water's Edge. Coordination of design and engineering will be critical between the Water's Edge projects and the upland development of Port Gardner Wharf which is being led by Maritime Trust Company (MTC). These guidelines recommend character, quality, and standards for design teams working on the Water's Edge in order to create a very special and unique new community.



Sailboat Sleek Functionality

Design of the Water's Edge shall provide a character that is spacious and marine-focused. Like a well-designed sail boat, materials and design shall derive its style from clean lines and sleek functionality using materials appropriate to the marine environment. This



Sailboat Clean Lines

will provide a timeless quality through proven materials and a high level of design.

Each of the three water edges of the site (south, west and north) shall provide public gathering places such as plazas and areas for viewing and seating.

In the Craftsman District's frontage of the Water's Edge, commercial marine operational activities shall be designed for and managed to allow pedestrians to safely pass, and boat yard systems to function efficiently.



Bench with Marine Character



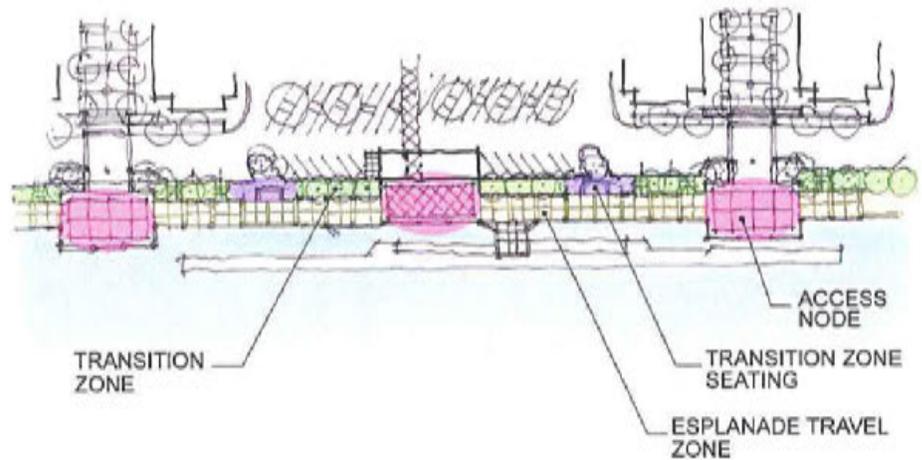
Marina Railing

All development along the Water's Edge must meet shoreline requirements and regulations. These guidelines for design may require site specific adjustments to meet shoreline and/or other regulatory requirements.

The Water's Edge provides continuous public access around Port Gardner Wharf consisting of the pedestrian



walkway (Esplanade) as well as key connective spaces (nodes) and passages to boat moorage, public plazas, and commercial areas. As the primary public amenity for pedestrians at Port Gardner Wharf, the Esplanade at the Water's Edge will provide a strong unifying element connecting a variety of areas and districts each designed with their own specific qualities. The spine of the Esplanade is a 20' wide walkway that includes a 12' minimum clear travel way. This walkway will use repetition and its inherent linear quality to provide a strong and consistent design



Esplanade Zones and Nodes



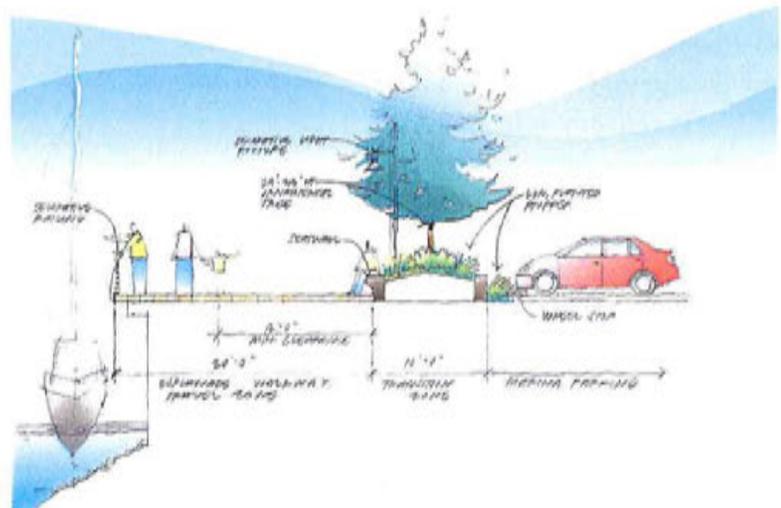
An Esplanade

Between the Esplanade walkway and upland developed areas, a 10' wide transitional zone will be created. This space will be used for planting, seating, grading, or additional amenity space for aesthetic enhancement. It may also be used for marina services and outdoor restaurant seating. The transitional zone may be designed to reflect the unique characteristics of the upland district. Some of the unique areas bordering the Water's Edge may contain a variety of gathering places such as plazas and areas for viewing and seating that can be merged with the transitional zone area. Unlike the Esplanade walkway, the treatment of these transitional spaces will vary in their use, design, and materials reflecting the district's unique characteristics.

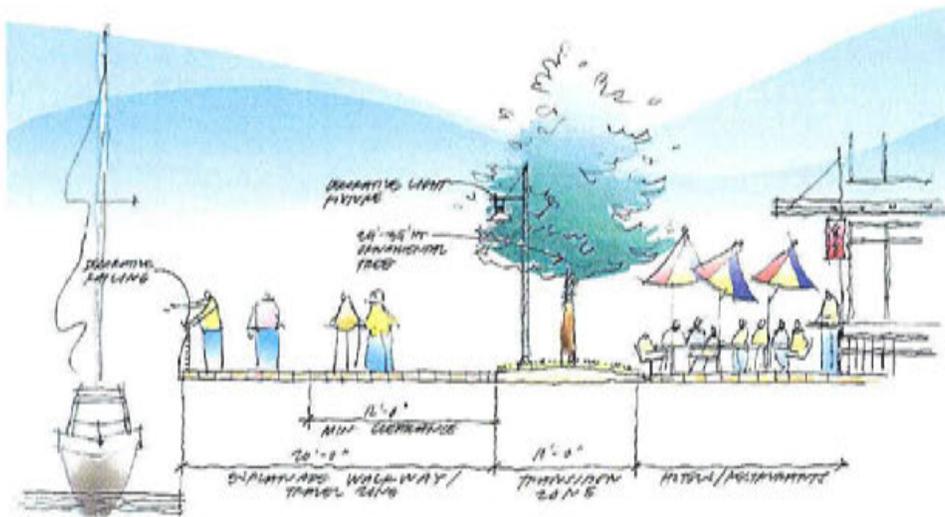
To the south, the 14th Street Marina transition zone will include a planted buffer along

the parking lot. At the West End, the zone will create a transition between the Amphitheater and the tall ship moorage. It will also be the seam between the hotel and public Esplanade. At the Captain's Walk and 12th Street retail area, the transition zone will accommodate grade changes and restaurants with patio seating. Access nodes to the proposed 12th Street Marina will also be included. At the Craftsman District to the north, the transition zone can be used for buffering the light industrial activities.

The following characterizations of the Water's Edge Esplanade areas give a general sense of the distinct qualities. Other Sections of these guidelines should also be consulted to gleam the overall character that will be es-



The South Esplanade



The West Esplanade

established through the streetscape, plazas, and through the developed sites and buildings in each district.

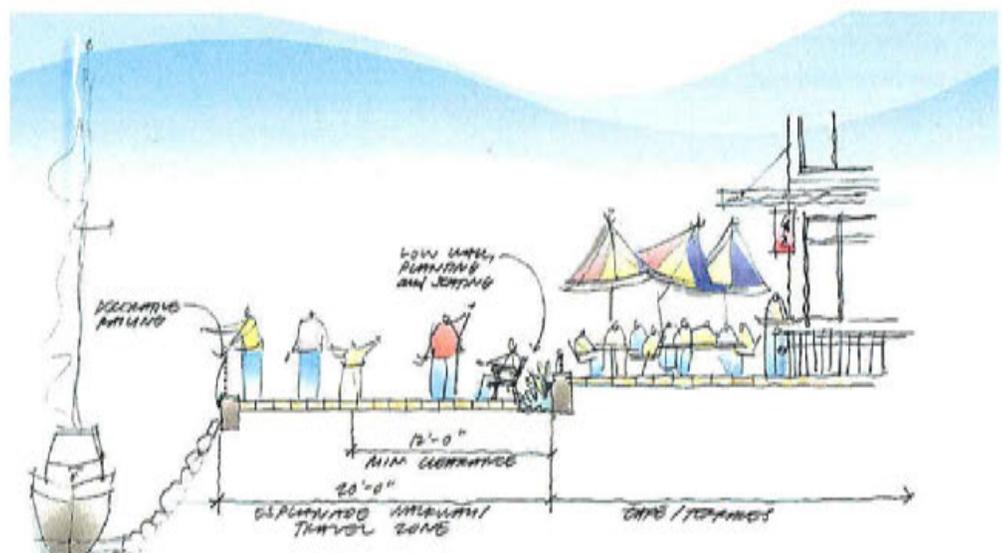
**South** – The South Esplanade borders the 14th Street Marina which has slips for permanent and visiting boat moorage. A large parking area services the marina and runs the entire length of the South Esplanade. The transitional zone between the 20' Esplanade and parking area will be characterized by a low planted buffer, integrated seating, transitional pedestrian access ways, and marina service buildings and facilities. Access from the Esplanade to the boat docks will be through secure access gate structures with gangways to the docks and slips.

**West** – The West Esplanade will border the area that is envisioned for development of hotels and restaurants, and the public Amphitheater at the terminus of the 13th Street corridor. The Amphitheater is centered along the length of the West Esplanade. This area will also include access to the Tall Ship Moorage. The West End of the Wharf will be characterized by a high level of integration and interconnectivity between the semi-public spaces of the hotels and

restaurants and the Amphitheater, the Wharf's most prominent open space. The transition zone of the Esplanade will be seamless with these developed areas, providing connections and gathering places. These will correspond to the dominant uses and locations where seating or gathering areas are directly adjacent to the Esplanade travel lane. The environment should use materials of the highest quality, and low in height to maintain openness for views and sun exposure. More detailed guidelines for the Amphitheater

and other public spaces are included in Section 3B Plazas & Open Space.

**North** – The North Esplanade serves the new 12th Street Marina, the future commercial development of the Captain's Walk and retail district, as well as the Craftsman District marine related businesses and boat ramp. Further to the North is the 10th Street Marine Park which will be tied to the North Esplanade with a paved trail. As the North Esplanade moves through these various districts, the unique character of the areas will be expressed through design in the transitional zone between the Esplanade walkway and future development. As the major commercial retail area, the Captain's



The North Esplanade

walk will take advantage of views with seating areas for cafés, restaurants, and windows. Grade changes on this north edge should be used to enhance these views and create terraces, walls and planting.

The Craftsman District is primarily a marine-based commercial and light industrial area that will exist independently from the pedestrian Esplanade. Here, the transitional zone will draw on the Craftsman District characteristics through the use of elements which are more industrial in character. This zone should provide some buffering and containment in areas where the district's uses may be incompatible. Design should reflect the haul out's interruption of the Esplanade by providing strong visual awareness, through surface materials and lighting of the area, and by integrating safety measures. At the 10th Street Marine Park, the characteristic elements of the Port Gardner Wharf Esplanade diminish as the connecting path becomes more naturalistic.

Access to the 12th Street Marina will be from both the Esplanade along the 12th Street retail area of the Captain's Walk District and from the Marine Park to the north. Although the access points may take on some characteristics of their districts, the gangways and docks will be the same as with the 14th Street Marina.



Esplanade Access Node

## Esplanade and other Pedestrian Walkways

Design Intent – Water's Edge pedestrian walkways will include unobstructed durable surfaces for the Esplanade, marina access ways, and connective areas for



A Multiuse Travel Zone with a Transition Zone Buffer

other associated public amenities such as the Amphitheater. Design of the main walking area called the 'Esplanade Travel Zone' will provide a multiuse path that serves as a strong linear framework and consistent edge to the waterfront environment. A rectilinear plane of repetitive scoring patterns or unit pavers will provide a consistency and unity. This pattern should only be interrupted in areas called 'Esplanade Access Nodes'. These lateral walkways provide pedestrian connections that tie into the Esplanade from upland areas. They will intersect the rectilinear Esplanade Travel Zone walkway pattern with special materials, colors, and patterns that reflect the characteristics of the associated upland district. The 'Esplanade Transition Zone' is the area on the land side of the Travel Zone that provides area for seating, low walls, planters, plazas, and other public or semi-public amenities.



Esplanade with a Wide Travel Zone

**Accessibility** – Pedestrian walkways shall comply with Chapter 11 of the ADA Handbook and all other accessible requirements and guidelines of the Access Board, State of Washington, and City of Everett.

## Marina Parking

**Design Intent** – While parking at the Port Gardner Wharf is predominantly on-street and integrated into garages, 14th Street Marina parking is part of the Water's Edge and is included in this chapter of the Design Guidelines. The parking for the marina is an important area for design consideration since it is a large area that will be viewed from upland residences and businesses, and will be traversed by all accessing the Esplanade at the south edge of the wharf. Its design should be conceived as a large plaza with well defined clear routes of travel to the Esplanade and marina. The visual impact of parking must be minimized with the use of varied paving materials, planting, and other design elements.

Access to the Esplanade from the marina parking will be clearly demarcated and include well defined pedestrian access ways (Access Nodes) accented with specialized materials, colors, and patterns. The primary nodes will lead through the parking and to marina services, view points, and dock access areas. The design treatment of these pedestrian access ways will help to reduce the

visual impact of long expanses of parking and will help in way finding for visitors and boaters.

Parking at the marina shall be separated from the Esplanade pedestrian walkway by the transition zone. This zone will include a low barrier such as a curb, low wall, or planting bed, except in areas for pedestrian access connections. On the upland side of the parking area (north edge) a low wall may be needed for grade change. The grade change and planting at the top and bottom of the wall will help to mitigate the visual impact of cars from upland residences.

Marina parking must accommodate areas for drop off and pick up. These should be designed as part of the

Access Nodes and near marina service and marina commercial buildings. In selected locations, bollards will serve to control vehicle access, but allow pedestrians to access the Esplanade and other Water's Edge amenities. In some areas, removable bollards will be needed to allow fire and maintenance access.



Defined Access Node through Parking

**Special Event Parking** – Parking for special events such as Market and Amphitheater events is predominantly a challenge of event management, but design must play a role in anticipating how the flow of cars and pedestrians occur to and from the marina parking area, if used for such events. Creating safe and clear way finding for access to and from the event areas is required. Access shall also include safety and convenience considerations for those with limited mobility.



**Parking Layout** – Parking stall dimensions and aisle widths shall conform to City of Everett ordinances, standards, codes, and regulations unless otherwise approved by the City.

**Parking Materials and Finishes** – Marina parking will be concrete, asphalt or permeable pavers except at pedestrian access ways. Wheel stops, curbing, or bollards are required to protect all planted areas. Vertical curb and gutters are required rather than rolled curbs.

**Parking Access Ways** – Parked vehicles and curbs or wheel stops must not impede pedestrian connection ways through the lot to the Esplanade. Boaters will often be carting supplies and need unobstructed and barrier free passages between parked cars.



Thoughtful Transition of Materials

Pedestrian connections shall be integrated into the parking lot layout to provide safe, clearly visible, and unobstructed points of access to the Esplanade and Marinas. Dimensions, materials, colors, and textures shall be as recommended in the Pedestrian Walkway standards.



Trowel Edge and Broom-swept Concrete

**Landscape Requirements** – Parking lot design landscape requirements shall meet the City of Everett standards, codes, and regulations unless otherwise approved by the City. In addition, marina parking shall use the plant materials listed in the Water's Edge Plants and Planting section of these design guidelines, or plants with similar character and habitat



Parking Lot Constructed of Permeable Pavers

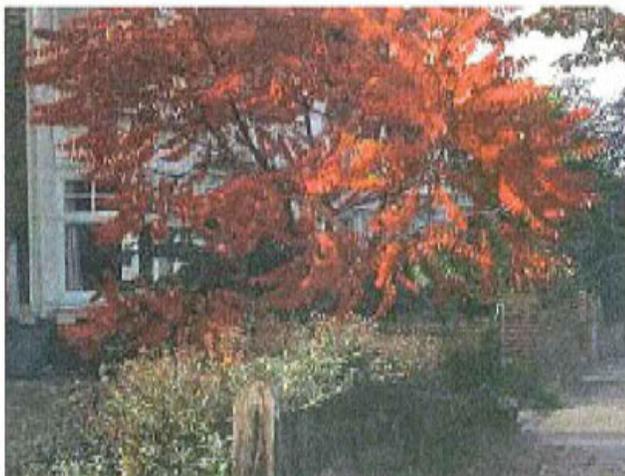
## Plants and Planting

**Design Intent** – Planting along the Water's Edge will be a major design element for enhancing character and the quality of place. Plants will define the transition zone of the Esplanade and the access nodes, but will not be located on the water side of the Esplanade. Layout and selection should reinforce and define spaces, transitions and edges. Species should be clustered and large drifts and groupings rather than being placed piecemeal with excessive variety.



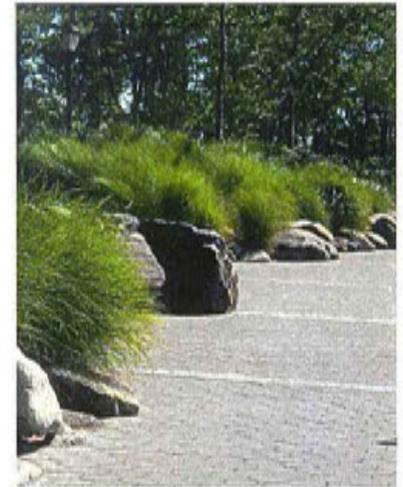
Planting at the Top and Bottom of the Wall will Mitigate View to Parking

Plant palettes that recall historic waterfront communities or complement the natural shoreline flora of the Pacific Northwest are preferred. In all cases, plants shall be hardy to the marine environment and emphasize the



Sumac

unique qualities of their context. Planting shall be selected and placed to enhance rather than obstruct views from Port Gardner Wharf upland development. Selections shall be low-water use, grouped in associations with similar sun, soil and water requirements. All planting must be irrigated.



Plant in Large Drifts

**Trees** – Deciduous trees shall be used in the transition zone of public plazas, cafés, dining areas, and other gathering places just off of the interior side of the walkway. Trees will be used to reinforce pedestrian connections to the Esplanade, and to define and provide shade for seating areas. Trees shall be placed in bosques and alle's at pedestrian connections, and informal groves at transitions to the water's edge. Trees in view lines of upland residential areas should be columnar or branched clear to 8' ht. and have an open canopy.

**Understory Planting** – Shrubs, groundcovers, grasses and perennials shall be used in the transition zone on the Esplanade interior edge plazas, cafés, dining areas, and other gathering places. Plants along the transition zone will be used to provide a buffer for parking and service areas. The plant palette shall be refined and elegant. Understory plants along the transition zone should be designed in simple drifts incorporating ornamental and native-like species of shrubs, grasses and groundcovers recalling form and texture of a natural shoreline.



Refined Plant Palette of Grasses

# The Water's Edge

Layered planting should provide four season interest. Focus should be on texture, foliage and fragrance rather than flower. Lawn should only be used in areas for passive or active recreation and at transitions requiring circulation.



Urban Accessories 'Viper'

Tree Grates – Tree grates shall be used for all trees placed at ground level that are not part of a larger planting area. The surface of the tree grate shall be ADA compliant. The size is to be a minimum of 4' x 4', but 4' x 6', or 5' x 5' is preferred. Designers should explore the possibility of a custom designed tree grate for the Water's Edge to express and build upon the unique character of the Port Gardner Wharf. The tree grate should embody the design elements of Water's Edge with a refined and elegant patterning. It may abstractly reflect marine or shoreline characteristics such as ripples of light on the water or wind blown patterns of sand. If a custom grate is not used, a grate such as Urban Accessories 'Viper' is recommended.

Planters, Pots, and Boxes – Planters, plant pots, and planter boxes should be used in the Esplanade Transitional Zone plazas, cafés, dining areas, and other gathering places just off of the Esplanade.



Pots Must be Large

Planters shall be raised 12" to 24" with seat walls at the edges to protect plants from pedestrian short cut paths and trampling and also provide informal seating areas. Potted plants, if used, should be large. Planter boxes should be considered to add detail and interest to cafe' railings or exterior architectural features.

### Planting Integration

– Art, architecture, and signage shall have site specific integrated planting. Planting is to complement and enhance the design through color, fragrance, visual texture, and seasonal change. Plant selection and landscape architectural design must consider the plants context and ensure the mature size and character of plants are well integrated.



Planter Box in Railing

## Illumination

**Design Intent** – Water's Edge exterior illumination will provide light for the Esplanade, pedestrian and vehicular access areas, plaza spaces, the marina facilities, associated public amenities, and for accent lighting on special architectural and landscape features. Design will be special to the Port Gardner Wharf concept and character reflecting this unique urban maritime waterfront environment. Fixture types should convey an image that ties to the nautical theme through such elements as horizontal rings and bulb guards often found on fixtures in ships. The image and personality of the entire Water's Edge theme should be expressed not only through fixture type and location, but also through elements such as the incorporation of specialty logos or images on the lights, poles, or pole bases.



Nautical Inspired Fixture

**Dark Sky Requirement** - Fixtures along the Water's Edge will be selected that minimize light pollution. Per the development agreement, a dark sky is required to be maintained for nearby residents. This will also be beneficial for residents living upland of the Esplanade and marinas. Illumination must adhere to dark sky criteria as established by the City of Everett and per industry standards.

**Illumination Quality and Levels** – Light quality and level of brightness are very important along the Water's Edge. Well balanced color is required such as that produced by metal halide, halogen, or incandescent bulbs. Due to the undesirable color, no high pressure sodium lights are allowed. Footcandle levels shall meet the IESNA recommended standards for the specific uses along the Water's Edge. These standards ensure that lighting levels function in a manner that provides safe and clear illumination to motorists and pedestrians, and that the entire development is properly lit to ensure a safe and secure neighborhood. Lighting should not, however, bleed or overlap to create unnecessary brightness or glare. Low, shielded fixtures are preferred on the Water's Edge.

**Placement** - The placement of lighting fixtures should be discrete and unobtrusive so as to eliminate physical clutter and avoid glare into residences and commercial offices. Placement of lighting fixtures should also be done to facilitate maintenance such as changing lamps.



Use Low Shielded Fixtures on the Water's Edge

**Maintenance and Operation** - Lighting fixtures should be placed and designed to minimize maintenance and operation costs. This includes maximizing lamp life through the use of motion detectors and photo-voltage and timer switches. Where possible, lighting may serve more than one illumination function.

**Materials** – Exterior lighting will be constructed of materials durable for the marine climate. Fixtures should have vandalism protection where possible. This includes stainless steel, heavy gauge aluminum, and metals with a powder coat finish.



Lightform L209 Lighthouse Bollard

**Finishes** – Finishes will be durable and low maintenance. Finishes shall be stainless or powder coated.

**Colors** – For exterior light fixtures that are not stainless, finishes should be black or as approved by the Design Review Committee.

### **Types of Exterior Lighting:**

**Pedestrian Lighting** – For safety, security, and for aesthetics, pedestrian lighting is an important aspect of the Esplanade and associated spaces. Illuminated bollards and footpath lights will be used to illuminate the ground plane. Where possible, use either recessed, wall mounted, or free standing bollard light fixtures that keep the light low to illuminate sidewalks and pathways. Inset stairway and stair step lights are required on all stairs.

Pole mounted pedestrian lights, if needed shall be 12'-14' in height with light focused downward and shielded from the night sky and from residences. Pole mounted pedestrian lights may occur land side of the Esplanade at larger plaza's and terraces that overlap into the Esplanade Transition Zone. They also may be used at marina service and marina commercial areas. Accessories may include banner arms and baskets. The height of pole mounted lights is limited due to glare and the impacts on views.

Bollard lights will occur at wide pedestrian ways and plaza's adjacent to roadways and parking, for pedestrian safety, way finding, and ornamentation. Bollard lights serving as pedestrian street crossing control functions may be connected by



Bollard Light

chains. Other pedestrian illumination may include wall-mounted fixtures on buildings bordering sidewalks. These could double to illuminate flower beds/ planters as well. Special lights (such as the Selux 'Corral' column) may be used as a distinctive way finding element at the plaza's and esplanade bump-outs.

**Marina Access Lighting**- Lighting for accessing the Marina will be of a higher illumination level, especially at dock access points. These fixtures may be integrated into the access structures.



Selux Corral Column

### **Marina Parking Lighting**

Lighting for marina parking lots shall be the same fixtures to match the pedestrian lights. If pole mounted, the height of poles shall be minimized as much as possible and, if necessary, a higher wattage bulb shall be used. Lighting levels must meet local requirements, dark sky requirements, and IESNA standards. (Recommended: Architectural Area Lighting UCM LUM STR SLA RCK fixture)

**Accent Lighting** – Accent lighting will be used to emphasize special features such as fountains, sculpture, wall niches, planters, or trees for decorative effects along the Esplanade. Fixtures shall be inconspicuous and durable (Bega 2100 or 2221 Recessed Wall luminaries). Accent lights along the waterside of the Esplanade should integrate with railings, be recessed in walls or steps for pedestrian safety and way finding. Small scale accent lights (such as Bega's 8621 LED) can serve for way finding or be used themselves as special design elements. Accent lights may also serve multi-purposes where possible. They may also be used for illumination of signage.

**Building Lighting** – Building mounted lighting will be used to light walkways, terraces, and plazas in appropriate areas. Building lighting shall be designed to cast downward and be shielded to minimize glare in areas where it affects the Water's Edge. Because building



AAL UCM Pole Light

lights may be turned off, building lighting shall not be depended on exclusively for illuminating the Esplanade walkway and access points. In commercial areas along the Water's Edge, it enhances retail and restaurants to have spillover light. These areas should to cast light from windows onto the walkways for a distance of 10' from the building.

**Special Event Lighting** – Lighting used for special events along the Water's Edge may include decorative lighting for holidays and amphitheater events. This lighting shall be temporary and only used for the duration of the special event.

**Decorative Lighting** – Neon, running lights, and other types of colored or decorative lighting may be appropriate for signage, décor in buildings or as part of an art installation to enhance the Water's Edge.



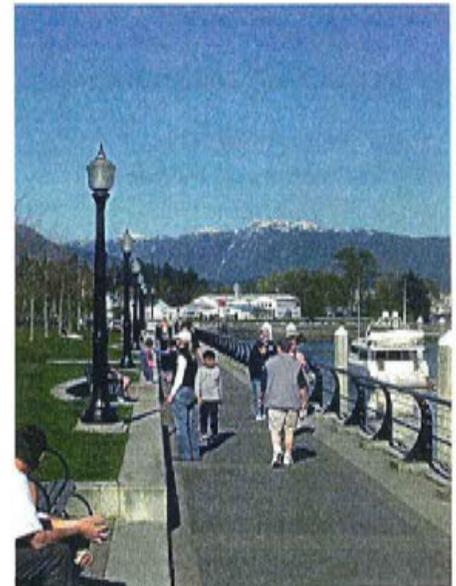
Decorative Illumination will Enhance the Water's Edge Character

**Street Lights** - Lighting for roadways and streets connecting to the Water's Edge shall not be higher or impede views from the second story residential windows. Tall cobra head and box highway type lights are not allowed. Fixtures should include twin arms with a light from one arm and either flower baskets, banners, or theme medallions suspended from the other. (Such as AAL's UCM SLA 17-2). See Section 3A Streetscape & The Pedestrian Network for more information on street lights.

### Water's Edge Exterior Furnishings

**Design Intent** – Water's Edge exterior furnishings will provide public amenities along the Esplanade. These elements will be integrated into the overall design of the Esplanade and help to establish a high quality and consistent urban structure. Design will reflect the maritime context through strong and simple forms with clean visual lines and well articulated details and connections.

The amount of exterior furnishings should be minimized and where possible, necessary elements should have multiple uses. Consolidation may include refuse and recycling containers, traffic signal controls, and bus shelters.



Integrated Esplanade Furnishings

**Materials** – Exterior furnishings will be constructed of materials durable in the marine climate. This includes steel, heavy gauge aluminum, and masonry. In some cases, wood that is well treated to hold up in the marine environment may be appropriate for furnishings in areas where vandalism won't be an issue.

**Finishes** – Finishes will be durable and low maintenance. Finishes for metals shall be stainless, galvanized, or powder coated. Finishes for masonry should be ground smooth rather than rough or split face. Finishes for wood shall also be smooth, but must also include appropriate protective sealing and treatments for longevity.

**Colors** – For exterior furnishings at the Water's Edge that are not stainless or galvanized, finishes should be black or a dark color.

## **Types of Exterior Furnishings:**

**Benches** – Benches should be integrated along the Water's Edge Transition Zone. They will be located in a variety of public areas that provide opportunities for views out to the water, people-watching, catching sun, waiting for others, and finding some semi-private space for reading and relaxation. When possible, bench design shall include integration into walls and planters. Free standing benches should be appropriate to the marine design aesthetic and use strong simple lines and forms (such as the Fairweather 'Thea Foss' series). Benches should be designed or selected to minimize water accumulation on the seating surface and deter abuse from skateboards or other vandalism.



Fairweather 'Thea Foss' Benches

**Trash receptacles** – Trash cans shall be located in public gathering areas, areas of high pedestrian activity, and areas that produce trash such as concessions and cafes. Design of the trash receptacle must relate to the marine design aesthetic with clean lines and form. Trash cans will be lidded and enclosed to keep out seagulls and rodents. They also must be easy to maintain and change bags. Where possible, they should be integrated into seating areas.

**Bollards** – Bollards shall be used to protect pedestrians from vehicles in parking areas and where pedestrians access the Esplanade and other public spaces along



Mooring Bollard

the Water's Edge. Bollards must relate to the marine design aesthetic of other elements. Bollards may be a potential element for integrated art or the reuse of wharf materials such as large steel piers or granite slabs. If not, bollards shall fit with the Water's Edge aesthetic (such as Landscape Forms 'Annapolis').

Bollards should be limited to locations that do not interfere with parking, deliveries, and other functions, but may be used in multi-purpose functions such as posts for fence enclosures or sidewalk planters. Bollards may also define parking areas.

Bollards will be both permanent and removable. Permanent bollards will define edges and entrances to pedestrian areas and control vehicular access. Permanent bollards may integrate illumination when placed in pedestrian areas. Removable bollards are to be placed in areas where emergency or maintenance access is required.



Large Bollards Define This Plaza

**Bicycle amenities** – Bike racks shall be placed in areas near the Esplanade where bikers may need to park their bikes and wish to walk, shop, eat or rest. They will also be used by residents and those who work at Port Gardner Wharf. Bike racks will also be needed near the marinas,



Bike Rack

for boaters and boat visitors. Access to and from the bike racks should be clear and unobstructed. Bike racks must be wide for resting the entire bike against the rack rather than single-poled. Bike racks are a great opportunity for a sculptural treatment.

**Kiosks** – Display of information about the Port Gardner Wharf, community events, maps, and other information will be consolidated at information kiosks. These should be located not in the Esplanade, but rather placed at strategic Access Node locations where pedestrians access the Water's Edge public facilities and marinas. Kiosks are to be designed as an identifiable reoccurring element. Design must relate to the area aesthetic and the opportunity for the kiosks to be sculptural art should be explored. Kiosks will include areas for permanent information such as an area map, and for temporary information such as a community events calendar. Kiosk size must be kept to a minimum to reduce visual obstruction. Information should be displayed at heights readable for adults and children, and for those with varying physical abilities.



Information Kiosk

## Fences and Railings

### Design Intent

– Water's Edge exterior fences and railings will provide safety and security for public areas, but also should be designed for compatibility with each



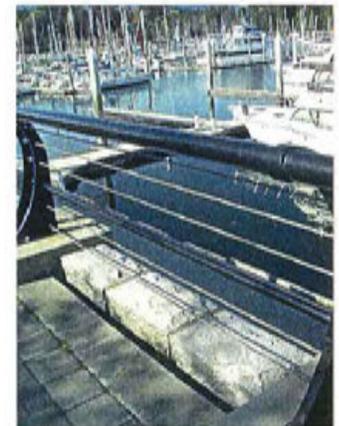
Esplanade Guardrail

other through form, materials, and finishes. The district aesthetic of strong simple lines and form should be reflected along the entire Esplanade. For upland areas such as along the interior edge of the transition zone, railing and fence design should be informed by the design guidelines of their particular district.

**Materials** – Exterior fences and railings will be constructed of marine durable materials including stainless steel, heavy gauge aluminum, and steel with the appropriate finish.

**Finishes** – Finishes will be durable and low maintenance. Finishes shall be stainless, galvanized, or powder coated.

**Colors** – For exterior fences and railings that are not stainless or galvanized, finishes should be dark, but may have light or bright accents.

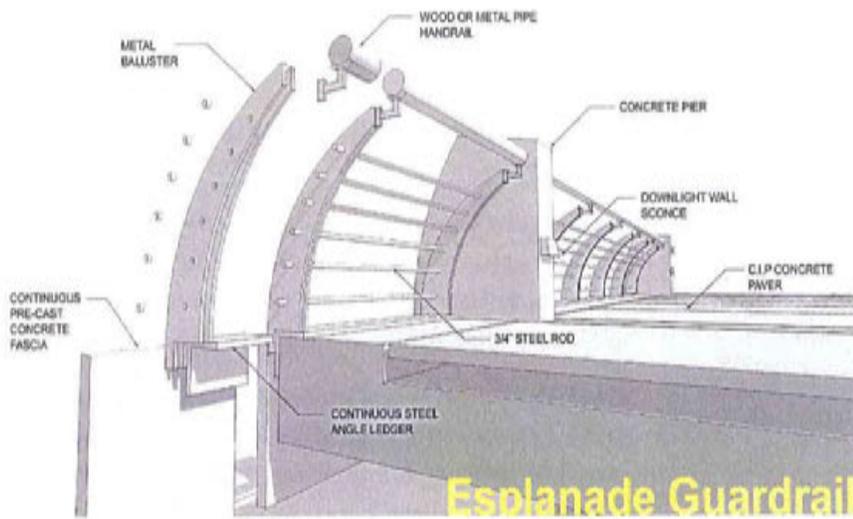


Durable Material and Finish

### Types of Fences and Railings:

**Fences** – At the Water's Edge, tall fences will be used only for screening and security at marina services buildings. For security areas, 5' to 8' tall fences will provide controlled access, but maintain visibility such that no one can hide behind any given area of the fence. For the screening of trash dumpsters and utility areas, a 6' high fence is required that will provide 100% screening when closed. In no case shall chain link fences be allowed except as a temporary solution for construction security. Low fences, 2' to 4' tall, will be used for defining areas such as cafes in the transition zone. Fences should have horizontal rails with a larger cap rail and vertical posts at regular spacing.

**Hand Railings** – At the Water's Edge, hand railings will be used for stairways, steep ramps, and other areas where a rail will help assist in balancing as one transitions grade changes. Hand rails must meet all required codes and standards for accessibility.



**Esplanade Guardrail** – In areas where there is a steep grade drop-off such as along the water side of the Esplanade or other potential safety hazard, guardrails will be used. Guardrails shall be meet local codes and standards. Along the Esplanade, the guardrail is a significant feature that creates a clear demarcation between the water and land. It's design should be unique to Port Gardner Wharf. It should be consistent throughout except possibly at areas of special emphasis like the tall ship moorage point, viewpoints, and dock access areas. Esplanade guard-rail posts must be centered on joints in the Esplanade walkway.

**Special Area Guardrails** – Areas of special emphasis like viewpoints, dock access areas, and the tall ship moorage point, shall be compatible with the Esplanade Guardrail using the same post spacing and height. Appropriate design variations will be incorporated to punctuate the special emphasis areas.



Special Area Guardrail

## Walls

**Design Intent** – Walls at the Water's Edge will provide definition, grade retention, and in some areas, may create transition areas to the water. Where possible, walls along the Water's Edge shall be designed as seat walls (12" to 24") to provide opportunities for public seating. Design will reflect the maritime environment through recalling historic wharf and sea wall design with materials, stone or masonry unit size, and joinery. Rockeries and rip rap may be part of the shoreline, but are not appropriate upland on the wharf.

**Materials** – Walls will be constructed of rock and masonry materials. This includes granite, concrete, and brick. Where possible, granite should reflect historic type and coloring.

**Finishes** – Wall finishes for rock will expose the natural material, but should be textured to various degrees to correspond to its application. Finishes for concrete walls should be smooth or lightly sanded and durable. Concrete shall have control joints with "V" groove joints or channels at frequent intervals. Protective coatings shall be required in areas with potential exposure to vandalism or staining. Other masonry materials must be long-lasting and low maintenance.



Water's Edge Seat Wall

**Colors** – For concrete walls, color should be integral or stained with a permanent, non-fading penetrating stain finish. Color pigment should replicate natural tones.



Concrete Wall Steps to the Sea

## Utilities

**Design Intent** – Utilities at the Water's Edge will service the public amenities along the Esplanade and the marinas. Although the design of utilities is often driven by utilitarian needs and constraints, their location and coordination with other elements will strengthen the overall design of the Water's Edge. Wherever possible, utilities will be consolidated and located below grade. On the Esplanade, this could be under pavers for relative ease of access. If utilities must be above ground, they will be screened or concealed. Utility lids and other exposed elements that can not be concealed or screened must be designed or selected to work with the area's maritime aesthetic.

**Location** – Locating exposed utilities is critical for maintaining



Utilities Could be Run Under Pavers

quality design treatments. All utilities that are not undergrounded, concealed, or screened must be located in coordination with exposed elements. They will be aligned and integrated with the layout of other design elements.

**Materials** – Utility grates, covers and other exposed elements will be constructed of materials durable enough for the marine climate. This includes cast iron, brass, steel, heavy gauge aluminum, and masonry.

**Finishes** – Finishes will be durable and low maintenance. Finishes for metals other than brass or cast iron shall be stainless, galvanized, or powder coated. Finishes for masonry shall be nonporous. Finishes with protective sealing and treatments for longevity should also be considered.

**Colors** – For exterior utilities that are not cast iron, stainless, or galvanized, finishes shall be dark to blend in with their surroundings.

### Types of Utilities:

**Utility Grates** – Grates such as trench drains and catch basin covers must be both functional and decorative. This is especially important in areas of high pedestrian use like along the Esplanade. Drain covers such as those with patterns can emphasize the connection to the nautical design concept (such as Urban Accessories 'Wave' or 'Viper').

### Utility Lids and Covers

– Cast Iron and galvanized steel utility lids and covers are potential elements for custom art and design. Designs should visually indicate the type of utility (electric, water, gas, etc.) and tie with the marine aesthetic. (For drain covers not custom designed, the Urban Accessories 'Nautilus' is recommended.) Concrete lids are to be placed in areas out of sight and out of travel ways.



Utility Lid

**Utility Cabinets and Boxes** – Cabinets and boxes for utilities must be placed in areas out of sight and out

of travel ways. Where possible, they also should be finished in dark earth tone colors or materials that blend into the setting.

**Utility Corridors and Access** – For utility banks and lines where future access may be needed, design of surface treatments must take this into account. Design should both enhance the area and use materials that can be easily replaced or seamlessly patched.

## Dock Access Facilities

**Design Intent** – As with many historic waterfront elements, dock access facilities can and should be designed to add to the aesthetic quality of the Water's Edge. Controlled access to the docks and emergency and maintenance vehicle access are important elements to coordinate for design along the Water's Edge. Design must be aesthetically unified with other aspects of the Esplanade and be compatible with the maritime environment. For these facilities, thought should be given to expressing details and connections.



Historic Dock Access Gate

**Materials** – Exterior access facilities will be constructed of materials durable enough for the harsh marine climate and heavy use. This includes steel, heavy gauge aluminum, and masonry. In some cases, wood that is well treated to hold up in the marine environment may be appropriate.

**Finishes** – Finishes will be durable and low maintenance. Metals should be stainless, galvanized, or powder coated. Finishes for masonry shall be nonporous and sealed with water-repellent sealers. Finishes for wood shall be smooth, but must also include appropriate protective sealing and treatments for marine climate longevity.

## Types of Access Facilities:

**Marina Dock Access Gate** – At the entry points to the docks and boat slips, security gates will be installed to control access. These gates must be designed to have an expression of the marine aesthetic. They should provide coverage for waiting and storage for carts. Visibility through the gate is important for safety and security. Coordination of design between the access gate and the components of the Esplanade and marina is required through material selection, alignment, surface patterning and dimensions.

**Marina Gangways** – Leading from the access gate entry points to the docks are gangways. Their slope will vary with the tides and must contain an all weather slip resistant surface. Guardrails run along both sides of the gangplanks. Coordination of design between the gangway and dock is required through material selection, alignment, surface patterning and dimensions.



Marina Gangway

**Marina Docks** – Docks must meet all the functional requirements including a slip resistant surface. Coordination of design between the dock and gangway is required through material selection, alignment, surface patterning and dimensions.

**Vehicle Access** – Access to the Esplanade and other Water's Edge open spaces and facilities must meet all maintenance and emergency access requirements. Ac-



cess points should overlap with points of access needed for pedestrians. Access should not alter design materials and finishes of the Esplanade. For emergency access areas, designers must work with the fire department to develop alternatives to yellow and red paint stripping.

Boat Ramps and Boat Lifts – Design of boat ramp water access and boat lifts along the Esplanade must be coordinated with safety requirements for pedestrians. Visual and physical controls should be incorporated to ensure safe pedestrian access. Noise signals should be minimized when possible.





## Chapter 4A: Building Site Design

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### Section A: Building Site Design

#### A.1 Site & Goals

- Building Site Design Development Goals
- Design Concepts
- District Site Design Elements

#### A.2 Building Site Design Standards

- Building Location & Setbacks
- Frontages
- Building Orientation
- Views and Visibility
- Corner Lots
- Building Access and Circulation
- Parking
- On Site Open Space
- On Site Landscape
- Parking Lot Landscape
- Site Lighting
- Sight Lines
- Sustainability
- Screening of Service Areas and Mechanical Equipment
- Flags and Banners
- Signage
- Electronics

Section B: Building Design General Standards

Section C: Building Types Character & Quality

## SITE & GOALS

### Building Site Design Development Goals -

The site design of the building considers the relationship of the building to its parcel of land and the larger context. The relationship between building and site must be integral. As an important element in the development of Port Gardner Wharf, it relates directly to the following goals:

- *Create strong relationships between site and buildings*
- *Create a pedestrian oriented waterfront destination for the City of Everett*
- *Use Sustainable Design Practices to create a healthy community*
- *Provide ample public access to the waterfront*
- *Maintain and Enhance view corridors per the agreement with the city*
- *Retain existing maritime uses where possible*

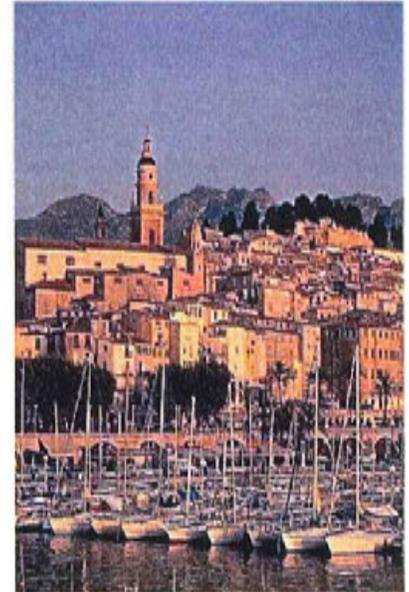


Buildings Must Be Well-Integrated Their Site

### Design Concepts -

The following design concepts guide the character of Port Gardner Wharf:

- Port Gardner Wharf will be a unique urban community with high-quality design.
- Maritime history, orientation, and other influences will continue to inform the site design.



Maritime Influences Inform Site Design

- District identity will establish a family of distinct areas each with their own character. Districts will complement the overall identity.
- Pedestrian oriented design will permeate the development, creating a lively public streetscape and waterfront.

### District Site Design Elements -

There are key elements and special site design considerations for each district-

- Gateway District – 13th Street Entry and the Gateway area along West Marine View Drive will provide an attractive, interesting, and active edge to the Port Gardner Wharf. Site design of Gateway buildings is important for framing the entry and portraying the character of the new community.
- Craftsmen District – Because this district maintains the Marine Commercial activities, special considerations of a building's site design must consider the movement and function of boat hauling, maintenance, selling, repair, and storage. Because travel lifts, air tools and other noise makers will be active during most of the working day, site design features must also be considered for noise attenuation.



- Residential Neighborhood District – Site design will help to characterize this quiet, multi-family residential area. Some commercial activities that are supportive to or compatible with the primary residential use may be included at street level.

- Captain’s Walk Retail Area - Site design will enhance this pedestrian oriented retail/ office area connecting the 14th Street Marina with the 12th Street Marina. The district also provides an east-west connection to the retail use on 12th.



Thoughtful Site Design Enhances Retail Areas

- West End - Site design for the Amphitheater, hospitality, and restaurants on this waterfront area will be important in defining the different uses and integrating service function.

- Water’s Edge - Esplanade, Marinas and other Public Spaces along the water will use site design for the public access area around the site. Design of site features for the Water’s Edge is covered in Chapter 3C.

## BUILDING SITE DESIGN STANDARDS

The following site design standards are to be applied to all areas of Port Gardner Wharf. Additional standards related to The Public Realm including Streetscape and the Pedestrian Network, Plazas and Open Space, and the Water’s Edge are covered in Chapter 3.

### Building Location and Setbacks -

- Districts and the Port Gardner Wharf preferred master plan define building type and location.
- No setback is required along sidewalk frontage except as may be needed for building design, architectural character, or as may be necessary to meet the requirements of these Design Guidelines or codes. In residential areas, front stoops, porches, and entries with stairs are encouraged.

### Frontages -

- Street frontages will be designed to complement the streetscape, the District character, and the building type’s use.



Residential Stoops are Encouraged

### Access -

- Although much access may be through garages, all buildings will have defined pedestrian entrances along all street frontages. A main entrance, defined as such by scale and architectural features, will be provided at the building’s street address frontage.

### Building Orientation -

- Views – Views are a critical factor in the site layout of many types of buildings such as residences, hotels and restaurants. Views to the water must be maximized through building orientation.



Views are Critical in Site Design and Building Orientation

- **Uses** – Building use will, to some extent, determine orientation on a site. Residences, hotels, and restaurants shall be oriented to maximize views to the water. Some offices may need views, but many should be laid out with consideration of comfort and ease of use for office workers. Retail streets will be focused on a strong retail streetscape. Marine related commercial will be laid out for operational considerations.
- **Solar orientation** - Buildings shall be laid out on site to maximize solar gain in winter and minimize it in summer. All buildings must have access to natural light and ventilation for users either with windows or sky lights.
- **Master Plan** - Building orientation will also be subject to meeting the building layout indicated in the Port Gardner Wharf Master Plan.

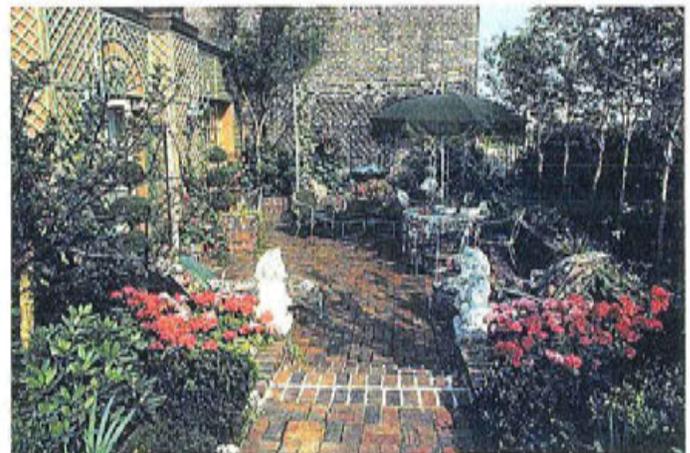
#### Views and Visibility –

- **View Corridors** – Buildings will maintain View Corridors at 13th and 14th Streets by preservation and enhancement. The view corridor shall be protected by limited building in these corridors. A view corridor of 80 feet along 13th Street Right-of-Way extends from the east entry at West Marine Drive to the west end of 13th Street at the Amphitheater. Buildings within the 200' shoreline jurisdiction will be limited to a maximum of 35 feet. A view corridor along the 14th Street Right-of-Way from West Marine View Drive to the marina area will be maintained. Enhancements will include corridor landscape and public open space treatments.
- **Residential Views** – Building site design will improve views from hillside residents to the east through

building location, high quality design, and by the siting of roof amenities such as planted balconies and rooftop courtyards. Views from new residential buildings at Port Gardner Wharf will be protected and enhanced by building orientation, tree type and placement, and

control of lighting. Other site design consideration for views may include locating of mechanical equipment out of views, and siting buildings so roofs are angled to minimize glare. .

- **Visibility** – Designing for safety, security and character of place will include maximizing visibility. Visibility should be maintained in all areas by eliminating high obstructions such as tall walls and fences that act as visual barriers. Ground level lighting is required along all walkways to increase safety and security at night.



Rooftop Courtyards Improve Views of Port Gardner Wharf

#### Corner Lots –

Special site design consideration will be given to buildings on corner lots. Where possible, site design should emphasize the corner through layout and design. If possible, main entrances to the buildings should be located at the corner. Most corners have bulb out inter-

section treatments which provide extra room for special surface and urban design treatments. These treatments must be coordinated with the building's site design.

### Building Access and Circulation -

- Vehicular – The main access into developed sites will include the accommodation of vehicle parking. In the residential and office areas, parking garages must accommodate all required parking. Site design will need to minimize the visual impact of parking while at the same time provide clear and safe access. The site will also include on-street parking on all streets.
- Pedestrian – Safe access for pedestrians will be an important element in all site design. Access must connect building entries to the pedestrian network of the streetscape, plazas, and other public spaces. See Section 3, The Public Realm.
- Bike – West Marine View Drive serves as a bike route to and from the site. It will connect with all streets at Port Gardner Wharf. These streets connect internally through the site, with the Esplanade serving as a main



Design Corners for Emphasis

bicycle route loop along the Water's Edge. All developed areas of the site must accommodate bicyclists by providing secure bicycle storage facilities. These must be integrated into the site or building with consideration of safety, ease of use, and aesthetic character.

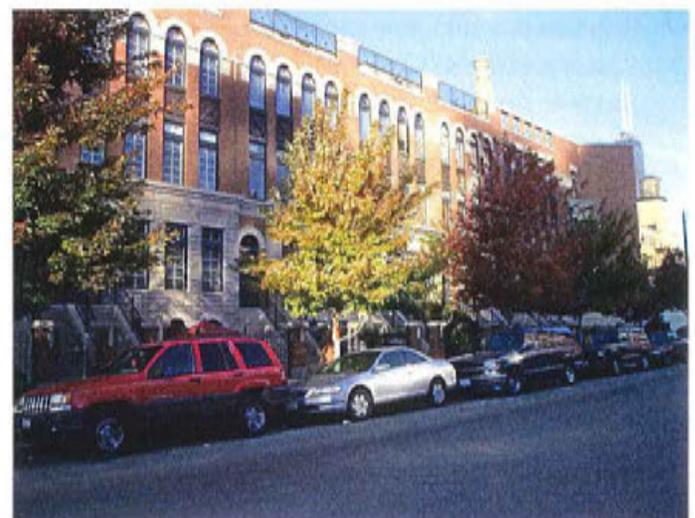
- Transit – Transit will access the site at 13th. Bus stops will be located near intersections by extending the bulb out into the parallel parking zone to accommodate a bus platform. Site design of parcels must consider the potential for transit facilities and access to and from these areas.

### Parking -

- On-street Parking – Included throughout most of Port Gardner Wharf, on-street parking will provide space for visitors and customers. Design of sites must coordinate location of building, parking lot, and garage access with on-street parking. Also, on-street parking must be coordinated with location of building uses. See Chapter 3 for guidelines related to on-street parking.
- Parking Lots and Parking Plazas – The 14th Street Marina parking lot, smaller parking plazas on 12th, and the parking lots in the Craftsman and Gateway Districts will provide space for visitors, customers, and workers. Design of sites must coordinate the orientation of the building with parking to minimize its visual impact, but still provide safe and efficient building access.



Design Building Access for Pedestrians, Bicycles, and for Vehicle Parking



Coordinate Building Site Design with On-Street Parking

- **Garage Parking** – Access to garage parking will need to be designed with both safety and visual appearance in mind. Garages must be underground or located so as to be visually diminished.
- **Shared Parking** – In mixed use buildings, shared parking should be explored to maximize efficiency of parking/ building square footage.
- **Curb cuts** – Site design shall maintain a continuous uninterrupted sidewalk by minimizing the impacts of driveways. Curb cuts shall not exceed 28' width for combined entry/ exits unless called out to a different dimension by the City Engineer. Sidewalk patterns and colors shall carry across driveways and crosswalks to show pedestrian crossing areas. Adjacent developments must consolidate (share) vehicular driveways wherever possible. Curb cuts may not be located near intersections per Everett standards.

#### On Site Open Space -

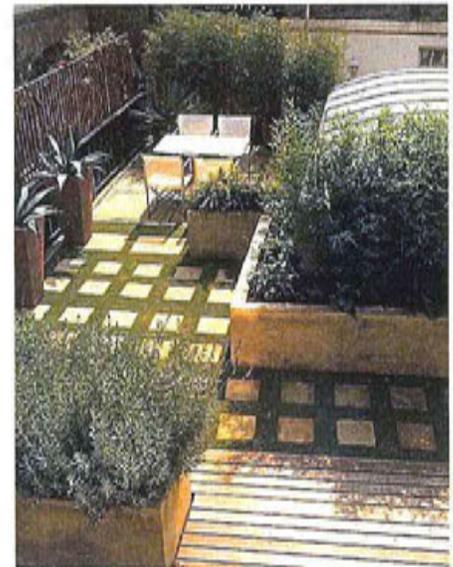
All residential buildings and commercial office buildings are required to integrate open space into their building sites. Common open space such as courtyards and pocket parks will supplement private open space such as balconies and patios in residential buildings. See Section 3B Plazas & Open Space.

- **Courtyards, Decks, Terraces, and Patios**– Some building types are required to contain courtyards, decks, terraces, and patios. In some areas, these spaces will be set at a higher grade to increase views. Total space provided in each area shall cover an area approximate to that shown in the master plan and as required. For major courtyards that are more public, the surrounding adjacent facades shall be “interactive”, meaning that a visual relationship



On Site Open Space

is established between outdoor and indoor spaces and that there is transparency to the façade. For more private spaces, design must consider the adjacent uses and provide privacy where appropriate through landscape buffers and architectural separation and screening.



Residential Terrace

#### On Site Landscape -

- **Locations of Planting** – Planting shall be used to enhance a site with an overall landscape architectural design and that provides variety and interest within landscaped areas. Landscape architectural design will include courtyards, plazas, and patios and, in some cases, it will include building frontage enhancements along the streetscape.



Planting Provides Variety and Interest

- **Plant Types** – Plants used for site enhancements shall conform to Chapter 3, The Public Realm. Courtyards, plazas, and patios shall be developed with plant-

ing per 3B Plazas and Open Space. Building frontages shall be developed with planting guidelines in 3C Streetscape & the Pedestrian Network.



Residential Courtyard

- Maintenance - If drought tolerant plant materials are not used, permanent irrigation shall be required. Where drought tolerant plants are used, temporary irrigation shall be provided for two years. Low maintenance plants should be used wherever possible. Gravel, river rock, pavement or similar non-living materials should not be used as groundcover substitutes.

### Site Lighting -

- Site lighting contributes to the character of the development, minimizes disturbance to housing on the bluff, and provides pedestrian scale lighting in parking lots and other areas.



Site Lighting Creates Character

- Lighting along streets, pedestrian walkways, and in plazas are covered in Chapter 3, the Public Realm.

- Lighting fixtures shall be limited in height and shall be shielded from

the sky and adjacent properties and structures, whether through exterior shields or through optics within the fixtures. Direction of light shall be downward.

- Accent lighting should be used to highlight building/ site entrances and special landscape features.

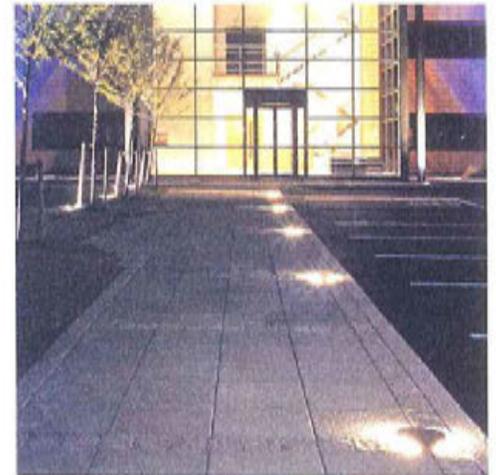
- Special lighting is encouraged during holidays and festivals, however, no flashing, moving, or traveling lighting will be permitted.

### Sight lines -

- Site design on the building parcel will adhere to required sight lines to ensure safe visibility at garage entries and driveway curb cuts.

### Sustainability -

- Sustainable site design practices are strongly encouraged at Port Gardner Wharf. LEED certification provides an excellent methodology and goal for incorporating green building principles into a project. LEED (Leadership in Energy and Environmental Design) Green Building Rating System® is a voluntary, consensus-based national standard for developing high-performance, sustainable buildings. LEED provides a complete framework for assessing building performance and meeting sustainability goals. Based on well-founded scientific standards, LEED emphasizes state of the art strategies for sustainable site development, water savings, energy efficiency, materials selection, and indoor environmental quality.



Lighting Should Emphasize Building Entries

Screening of Service Areas and Mechanical Equipment –

Visual separation is required between service and mechanical equipment areas, and all other areas of Port Gardner Wharf.

- All loading areas shall be visually screened by walls, tall fences, and plants. Service and delivery access to buildings will be out of view from pedestrian areas and from nearby residents and businesses.



Screens can be Attractive

- Trash Enclosures – Trash, garbage, and recycling dumpsters at pickup areas will be enclosed by fences or walls and accessed through gates. When not in use, gates will be closed. Views to these enclosures from pedestrian areas and from nearby residents and businesses will be minimized with the use of attractive screening and planting. All garbage bins shall be equipped with rubber lids to reduce noise impacts on adjacent residential and other uses.

- All mechanical equipment such as air conditioning units located on the ground shall be screened by a combination of masonry walls with plantings. Sound buffering must be used to reduce noise impacts. Mechanical units shall be located and screened in such a way as to direct exhaust and noise away from residences on and off site.

Flags and Banners –

Flags and banners attached to buildings are limited to the retail and marina areas of the Port Gardner Wharf. Flags may be displayed, but shall only be raised in morning and taken down nightly. Banners may be used on a temporary basis. .



Building Mounted Banner

Signage –

Business and place identification signs will be part of the architectural façade design. A hierarchy of signage will be used at the Port Gardner Wharf. Guidelines for signage is found in Chapter 2.

Electronics –

Security systems, TV dishes, cell towers, antennae, and all other electronic based equipment must be minimal in size and hidden from view.





Chapter IV: Building Design

# Chapter 4B: Building Design General Standards



Section A: Building Site Design

Section B: Building Design General Standards

B.1 Introduction

- Building Design Development Goals
- Design Concepts
- Building Design District Elements

B.2 Building Design General Standards

- Building Location
- Facades and Frontage
- Building Orientation
- Height and Massing
- Building Variation and Visual Diversity
- Building Setback
- Views and Visibility
- Parking Garages
- Canopies, Awnings, and other Overhangs
- Stairs and Ramps
- Corner Buildings
- Building Integrated Open Space
- Building Lighting
- Sustainability

B.3 Architectural Design Common Elements

- Architectural Ground Floor, Middle, and Top
- Roof Forms
- Entries and Doors
- Exterior Building Materials
- Window Glazing
- Finishes
- Colors
- Detailing
- Building Mounted Exterior Lighting

Section C: Building Type Character & Quality

## INTRODUCTION

### Building Design Development Goals -

Building design not only defines the space for the functional uses or program of a building, it is an essential component that establishes the character and quality of Port Gardner Wharf. The following goals are vital to building design for this community.



Architecture Creates Character

- Use architectural character to define the sense of place within a district and create a sense of community and neighborhood.
- Provide comfort and safety for residents, visitors, and workers through good building design.
- Build strong relationships between buildings and site.
- Maintain and Enhance view corridors.
- Create a pedestrian oriented destination.
- Use sustainable design practices to create a healthy community.

### Design Concepts -

- Port Gardner Wharf will be a unique urban waterfront community with high-quality design.
- Maritime history and orientation will be drawn upon for creative direction.
- District identity will provide unique areas that reflect building use and location.

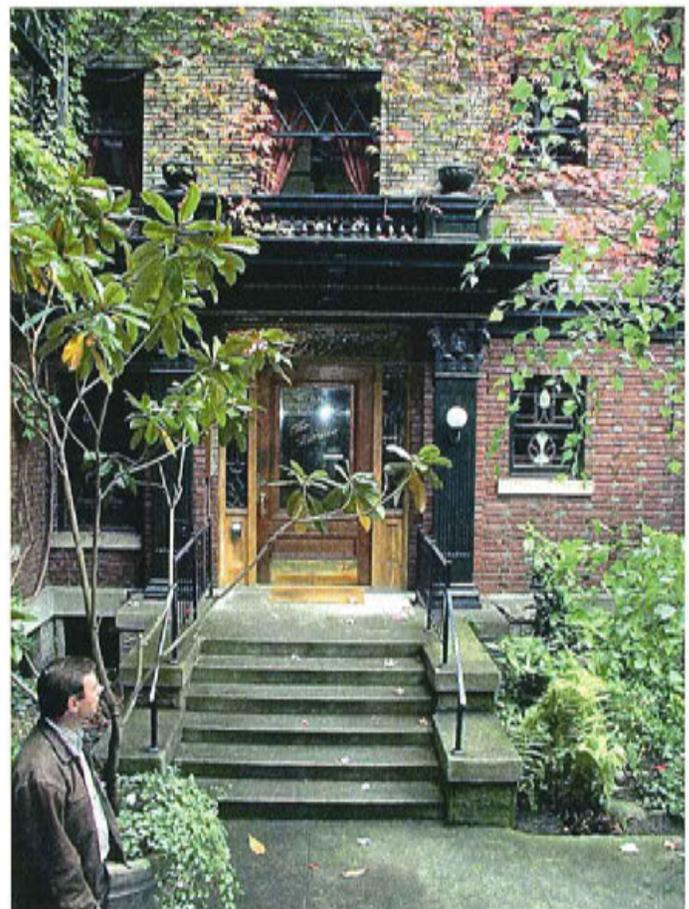
### Building Design District Elements -

There will be key elements and special building design considerations for each district-

- Gateway District – 13th Street Entry and the frontage along West Marine View Drive will use building design to provide an attractive and interesting community edge to the Port Gardner Wharf. Building design is important in giving the gateway character and an appealing face to the Everett community. The design of office and other buildings in the Gateway District will also create a campus of related architecture with amenities and special places for workers and visitors.

- Craftsmen District – Because this district maintains some Marine Commercial activities, special considerations of the building design must consider the light industrial uses of boat maintenance, selling, repair, and storage. Because travel lifts, air tools and other industrial noise will be occurring during most of the working day, building design features must also be considered for noise attenuation.

- Residential Neighborhood District – Building design will help to characterize a quiet, high-end multi-



Residential Character

family residential area. Some commercial activities that are supportive to or compatible with the primary residential use may be included at major corners. The architectural design will draw on a richer palette of materials and decorative detailing.

- Captain's Walk Retail Area - Building design will enhance this pedestrian oriented retail/ office area by defining great visible retail spaces energized by architectural design. Design will blend a rich palette of historically inspired and unique architectural elements with fundamental measures of successful retail and place making. The architecture will establish the framework for the variety of retail uses to succeed and unite for a strong retail district identity.



A Visible and Active Retail District

- West End - Building design for the Hotels and Restaurants on this waterfront area will be important in conveying the prime waterfront aspect and individual qualities of the different buildings.
- Water's Edge – Design of buildings for the Water's Edge includes Marina service facilities and retail buildings. These buildings along the water will use functional design to support the marinas and the public access areas around the site, as well as convey the maritime quality.

## BUILDING DESIGN GENERAL STANDARDS

The following general building design standards are to be applied to all areas of the Port Gardner Wharf. Specific design standards related to each District and associated building types are covered in Chapter 4C. These, and all other standards and guidelines, are in addition to all required building codes and ordinances.

### Building Location -

- Districts defined in the preferred Master Plan define building type and location.

### Building Facades and Frontage -

- Building facade treatment will be designed to create a pedestrian friendly presence and image by coordinating with the District's streetscape design. This includes compatible use and layout between the architectural façade with the streetscape's landscape, ground plane materials, lighting, furnishings, utilities, etc.
- Ground level building components such as doors, windows, bays, canopy structures, etc. must convey visual interest and reinforce the District theme. These architectural components will create visual and textural interest at eye level for pedestrians. Curtain wall facades are not allowed.



Coordinate Building Facade and Frontage with District Streetscape Design

- Buildings within pedestrian dominated areas should incorporate detailed architectural and decorative elements into ground-floor street-facing façades. These

elements include hanging baskets, signage supported by ornamental brackets, medallions, belt courses, plinths for columns, kick plates for storefront windows, prominent sills, tile work, etc.

- Access - All buildings will have defined pedestrian entrances along each street frontage. A main entrance, defined as such by scale and architectural features, will be provided at the building's street address. Access for some buildings will also include garage entrances. Garage entrances must be designed as integral elements and work aesthetically with the building's elevation.



Integrally Designed Garage Entries

Building Orientation -

- Views – Views are a critical factor in the site layout of many types of buildings such as residences, hotels and restaurants. Views to the water from buildings must be maximized through building orientation.

- Uses – Building use will, to some extent, determine orientation. Residences, hotels, and restaurants shall lay out buildings to maximize views to the water. Some offices may desire views, but many should be laid out with consideration of comfort and ease of use for office workers. Retail streets will be focused on a strong retail streetscape. Marine related commercial will be laid out for operational considerations.

- Solar orientation - Buildings shall maximize solar gain in winter and minimize it in summer. Architectural design must explore materials and methods to that achieve this goal and meet the requirements of the energy codes.

- Natural Light - All buildings must have access to natural light for users either with windows or sky lights. Building layout and orientation can help to maximize the availability of natural light and ventilation for residences and offices.



Design Buildings to Maximize Views and Natural Light

Building Height & Massing -

- Height Limits - Buildings must adhere to the permitted height limits.

- Massing - Building mass on very large buildings shall be broken down through a variety of design treatments.

Articulation will reduce the apparent bulk of buildings by breaking them down into smaller components that are consistent with a district's character, the Wharf's maritime vernacular, and the desired visual variety along the



Reduce Building Mass and Bulk

pedestrian and street faces. For additional information, see each particular building type in Chapter 4C Building Type Design.

- Large, flat, windowless, blank walls will not be allowed. Designers will be required to reduce the mass of large walls in creative ways such as expressing the structure or room modules. Bland walls longer than 100' should be treated with windows, bays, or recesses, or reduced visually in some other way. The purpose is not to create a regular rigid solution, but rather to encourage variety, visual interest, and pedestrian scale through the reduction of the mass of large building facades.

### Building Variation and Visual Diversity –

- Variation through unique architectural expression within the parameters of the District and building type guidelines, will help create a more dynamic urban waterfront character. On large buildings, it will reduce the apparent bulk of the building. Building variation between buildings will create a visually rich pedestrian experience.



Building Variation

- Visual diversity through a variety of massing, materials, fenestrations, heights, and roof treatments is required on all buildings.

### Building Setback –

- No building setback is required except as is necessary to meet codes and other requirements of these design guidelines, or as may be needed for building design architectural character or for creation of stoops, courtyards or terraces.

### Views and Visibility –

- View Corridors – As noted in the Site Design Section, buildings will maintain view corridors at 13th and 14th Streets.

- Roof Treatments - Building design will optimize views from hillside residents to the east through high quality design, height limits, and by providing roof amenities such as planted balconies, rooftop courtyards, terraces, and quality roof material on pitched roofs. Mechanical equipment will be screened and roof top color and materials will be selected to minimize glare and visual intrusion.



Roof Top Garden with Views

- On Site Views - Views from new residential and other prime buildings on site will be protected and enhanced by building design, as well as the site design considerations such as building orientation, tree type and placement, and control of streetscape lighting. Building design opportunities will maximize quality views through the organization of floor plan layout, size and location of window glazing, placement of decks, patios, and courtyards.

- Visibility – Safety and security measures will include maximizing visibility by eliminating architectural obstructions and visual barriers in areas of safety concern. Placement, type, and illumination levels of lighting are also required to meet safety and security standards.

### Parking Garages –

- Parking Garages are required to be part of the main building structure. They must meet all parking requirements and quantities as established by their use in accordance with all City of Everett Codes and standards.

- Parking garages must be “wrapped” and covered

by building usable space and amenities or located underground. No parking structure will be visible from any street, residence, or business, except at garage entries. Ventilation openings must be covered with louvers or grills composed with the building elevation.

- Safety and security measures are required in parking garages. Building design considerations for garages such as good lighting, no hiding places, surveillance, and well-defined pedestrian access through parking to elevators are all required in building garages.

#### Canopies, Awnings, and other Overhangs -

- Canopies, awnings, and overhangs will provide weather protection and serve to add a transitional scale to building facades. These structures should be provided at the access areas of all commercial/retail buildings. Such covers are required for main entrances of residential buildings and commercial office buildings. Canopies and awnings may project 4' to 10' over sidewalks, but stay at least 2' from the street edge. Cover for portions of exterior decks, courtyards, and patios are also encouraged.



Entry Canopy

#### Stairs and Ramps -

- Stairs should be minimized on retail, office, and other public oriented buildings. All buildings must be accessible. Stairs will be used in some residential housing types. Where there is a choice between stairs and ramps, small ramps that meet ADA standards are preferred to provide seamless connections between buildings access ways. Stairs and ramps must be slip resistant, clearly marked, properly illuminated, and meet all codes and standards for safety.

#### Corner Buildings -

- Special design consideration will be given to corner buildings at main intersections. Where possible,

buildings will enhance the corner through layout and design. If possible, main entrances to the buildings will be located at the corner. Corner buildings at bulb out intersections should incorporate special surface and urban design treatments that are coordinated with the building's design.



Architecturally Enhance Corners at Main Intersections

#### Building Integrated Open Space -

- Building design will accommodate open space requirements per code and per these guidelines. Courtyards, terraces, balconies, and patios must be integrated with the building architecture. See 4A Site Design and Section 3B Plazas & Open Space.

#### Building Lighting -

- Lighting on the exterior of buildings will contribute to the character of Port Gardner Wharf. As with all lighting on the site, lighting glare disturbance to residences is not allowed.

- Illumination will adhere to dark sky requirements through fixture type, optics, height, and shielding. Direction of light shall be downward.

- Building light design shall complement other design elements used throughout the District.

- Accent lighting should be used to highlight building entrances and special architectural features.

- Decorative lighting on buildings or in windows is

encouraged during holidays and festivals, however, no flashing, moving, or traveling lighting will be permitted.

## Sustainability -

- Sustainable Building Design is strongly encouraged at the Port Gardner Wharf. As with a building's site design, LEED certification provides an excellent methodology and goal for incorporating green building principles into a project. LEED (Leadership in Energy and Environmental Design) Green Building Rating System® is a voluntary, consensus-based national standard for developing high-performance, sustainable buildings. LEED provides a complete framework for assessing building performance and meeting sustainability goals. Based on well-founded scientific standards, LEED emphasizes state of the art strategies for sustainable site development, water savings, energy efficiency, materials selection, and indoor environmental quality.

## **Architectural Design Common Elements -**

The following common architectural design elements will establish a consistent general quality and character



Base, Middle, Top on Historic Buildings

throughout the Port Gardner Wharf. District specific and building type specific architectural design elements, found in Chapter 4C Building Type will give each building a distinct quality that enhances a district's uniqueness.

## Architectural Ground Floor, Middle, Top -

To ensure that buildings within Port Gardner Wharf display the greatest amount of visual interest possible, the following standards will apply to buildings with three or more stories.

- **Ground Floor** – Most buildings should have a distinct “base” at the ground level that emphasizes the edge of the streetscape. A base shall be defined through articulation of a distinct material or through a change in color or texture. The street level should have the richest collection of materials to create visual and textural complexity and interest at eye level. On masonry buildings, a granite or brick (or masonry cladding) in some form on walls, pilasters, or a plinth is strongly encouraged. Ground floor treatments must also include one or more of the following: windows, details, canopies, stoops, bays, overhangs, artwork, masonry strips, and/or cornice lines.



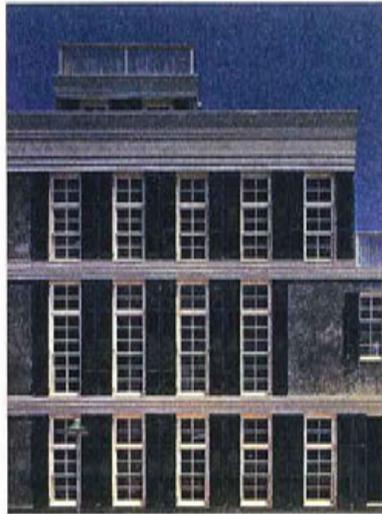
Ground Floor Base Created by Concrete Stoops

- **Building Middle** - The mid section of a multistory building should include elements such as windows, balconies, and in some cases may include signage. Upper floors shall be articulated both vertically and horizontally at street and main entry elevations. Verti-

cally, the floor height will be expressed through a band of color or material. Horizontally, a rhythm of materials, windows, or structure must be organized and expressed to correspond to the ground floor treatments. No blank wall facades shall be allowed at street or main entry elevations in any district.

- **Building Top** - Buildings will include a "top" or roof line treatment that has a distinct outline, such as parapet, cornice, or other architectural termination. The

topmost portion of a building should visually terminate with a change of materials, color, or pattern, a slight step back, brackets supporting an overhanging roof, a projecting cornice line, or with the eave of a pitched roof line. Extended parapets will include color, material, or pattern change with a distinctive cap along the top. Projecting cornices will include reveals and possibly some decorative detailing to capture shadow lines. Exposed sloped roof eaves must create a prominent edge when viewed from ground level against the sky. Brackets supporting an overhanging roof should be expressive architectural features.



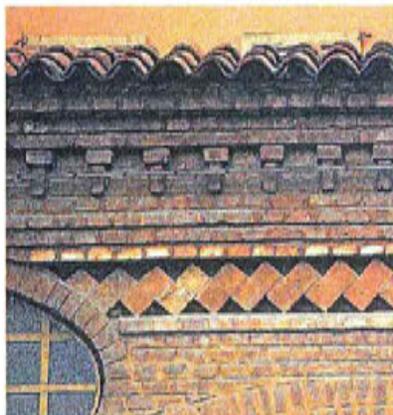
Building Middle

### Roof Forms -

Roofs are required to have a distinctive profile and appearance.

- Buildings containing predominantly residential uses with pitched roofs over 6:12

should have dormers with windows, intersecting roof forms, or roof decks that break up the mass of the roofs.



Decorative Roof line

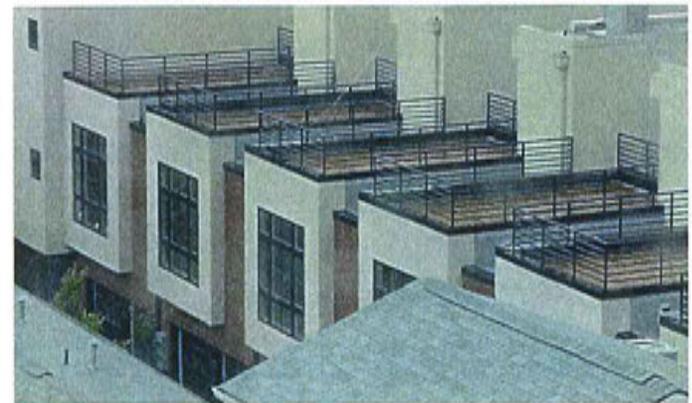
- Roof forms, if pitched, shall be no less than 3:12 or more than 8:12. If not pitched, they should be stepped and terraced. Shed roofs may be pitched 1:12 to 3:12. Barrel vaults and other forms may be allowed in some districts.



Prominent Projecting Eaves

- All mechanical equipment located on the roof shall be concealed. They should be integrated into the roof form, or covered by a structure, an extended parapet, or other shielding around the equipment. Painting rooftop mechanical equipment or surrounding them with fencing will not generally provide enough concealment to meet this requirement.

- Flat, unembellished roof lines are not allowed on building elevations.
- Roof top decks with integrated landscape are strongly encouraged.



Roof Decks

## Entries and Doors –

- All entries should include weather protection and should be visually emphasized through a concentration of expressive architectural materials, details, and decoration, and through entry lighting.

- Major entrances to buildings must be made prominent and welcoming not only with details and lighting, but also by being either recessed or having an overhang, canopy, portico, or porch.

- On larger buildings and commercial buildings, main entries should also include a clerestory, ornamental lighting fixtures, glass window(s) flanking the doorway and a large or double entry door. Large entries may also include signage.

- Entry surfaces for main entries that are not in the sidewalk should also use stone, masonry, or tile paving for an ornamental building name or address.

- Brass or stainless steel hardware that relates to the Port Gardner Wharf marine nautical aesthetic is recommended for entry ways and doors.

- Except for some residences which may include stoops, all main entries should be located on the ground floor and face the main street or prominent corner.

## Exterior Building Materials –

Exteriors will be constructed of quality materials durable for longevity in the marine climate. Durable materials add a dimension of timelessness and allow communities to evolve over time. Materials should convey a District's and building type's unique character and identity.

- Acceptable materials include masonry, coated steel, heavy gauge aluminum or aluminum alloy, and wood

that is well treated.



Durable Materials Allow Communities to Evolve

- Masonry should be stone (such as granite), brick, or high quality concrete. Cladding may be acceptable. Ground face block may also be acceptable in marine commercial and service areas.



Masonry Wall

For pitched roofs, slate, metal, or a composite material may be used. Color, texture, and unit size of composite roofs must have an appropriate maritime or natural look.

Composite exterior skin and roofing materials may be used if color, texture, and unit size are compatible with the district and building type's aesthetic.

## Window Glazing –

- Tinted, dark gray, green, mirrored or reflective glass or film shall not be permitted on any building.



Storefront Entry

### Finishes –

Finishes will be durable, low maintenance, and must fit with the Port Gardner Wharf aesthetic.

- Steel Finishes shall be stainless, galvanized, painted, or powder coated.
- Masonry Finishes shall be smooth or lightly textured.
- Wood Finishes shall include appropriate protective sealants for longevity.



Visually Interesting Details- Decorative and Functional

### Detailing –

• Building details, whether decorative or functional, must be thoughtfully designed to convey visual interest and reinforce the aesthetic quality of Port Gardner Wharf. Relating to the maritime aesthetic, details must express and celebrate an efficiency of design. Details should emphasize durable connections between materials. Detailing is a key component that expresses the unique characteristics and qualities of each building type. It will be a focus in Chapter 4C for each building type.

### Building Mounted Exterior Lighting –

Exterior lighting that is mounted to buildings shall be coordinated with the building's architectural design and character as well as the character of the District. Site lighting will be as described by District in Chapter 3, The Public Realm.





Chapter IV: Building Design

# Chapter 4C: Building Type Character & Quality

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Section A: Building Site Design

Section B: Building Design General Standards

Section C: Building Type Character & Quality

C1 Introduction

- Building Types per District
- Building Type Standards

C2 Building Type Character & Quality

- Introduction
- Marine Related Retail
- Marine Related Office
- Light Industrial Marine Services
- Marina Services
- Commercial Retail
- Commercial Office
- Hotel
- Restaurant
- Mixed Use
- Residential Flats
- Townhouses
- Lofts
- Live/ Work Residential

## INTRODUCTION

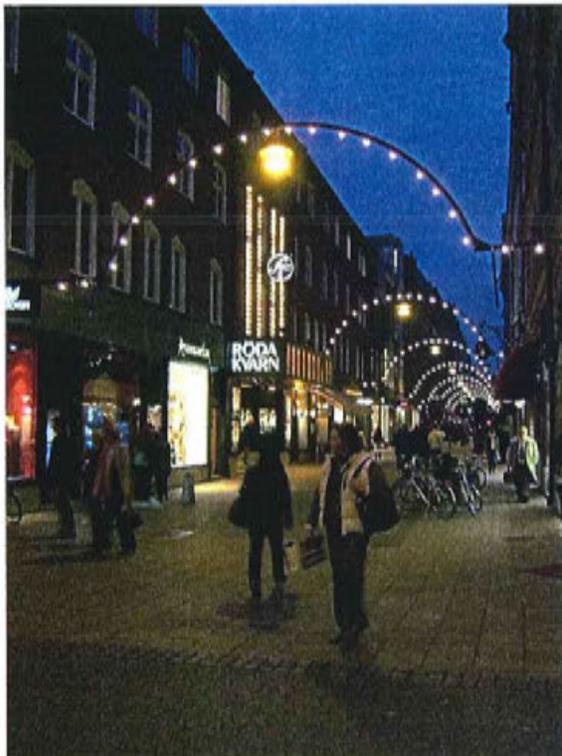
Building Design will both adhere to the general standards laid out in section 4B Building Design, and reflect the District's and building type's unique character and quality which is covered in this section. The following guidelines cover specific guideline standards for each building type. Each building type at Port Gardner Wharf will have certain standard characteristics that are specific to its use and location. The common standards are summarized in Table 4C-1 Building Type Standards. The most unique characteristics and qualities of each building type are elaborated upon in the section which follows - Building Type Characteristics.

### Building Types Per Districts -

#### Captain's Walk/ 12th Street Retail District -

Building types in the Captain's Walk/ 12th Street Retail District include the following:

- Retail (Sale of goods and merchandise)



Mixed Use and Retail

- Office (Small businesses and professional practices predominantly consolidated along side and above retail. If independent office buildings are developed, they will also follow these guidelines)

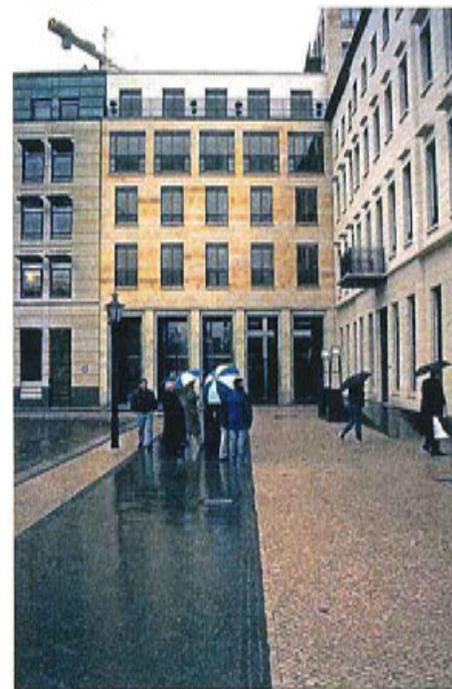
- Mixed Use (Ground floor retail with residential flats and/ or office on the upper floors)

- Live/ Work (Combination of residential with studio or office space)

#### Gateway District -

Buildings in the Gateway District include:

- Commercial Office (Offices and office support businesses)



Gateway District Will Include Offices

#### West End District -

Buildings in the West End District include:

- Retail (sale of goods and merchandise)
- Hotels (With potential conference facilities)
- Restaurants



West End District Includes Waterfront Hotels and Restaurants

## Craftsman District –

Building types in the Craftsman District include the following:

- Retail (Sale of boats and boat related products)
- Office (Maritime businesses consolidated within light industrial and retail commercial buildings)
- Light Industrial (Marine services buildings related to boat repair and servicing)



Craftsman District is Home to Maritime Business

## Water's Edge District –

Marine related buildings in the Water's Edge District include:

- Marina Service Facilities (Bathrooms and Laundry facilities for boaters)
- Marina Retail (Sales of goods and potentially food service for boaters and visitors)



Water's Edge Includes Marina Service and Retail

## Neighborhood Residential District (Wharf's Edge)–

This district contains multifamily residential and potentially some mixed-use building types including the following:

- Mixed Use with retail, office, or residential on the ground floor and residential flats or offices on the upper floors.
- Residential Flats of multiple stories.
- Townhouses, some of which may be integrated with residential lofts.
- Lofts which may be mixed with flats or built in a townhouse configuration.



Residential Flats with Water Views

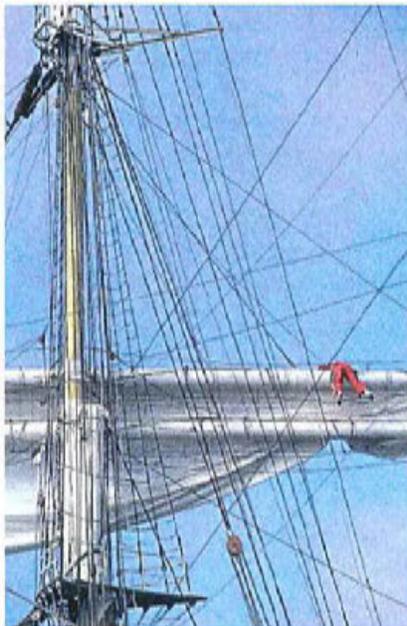
## BUILDING TYPE CHARACTER & QUALITY

### Introduction

In addition to the general standards laid out in 4B Building Design, each building type will strive to express its unique character and quality through the guidelines covered in this section. Buildings should reflect their District and their building type characteristics. The following guidelines cover some of the elements that will help to define a building's character as it will be customized for Port Gardner Wharf. Some guidelines are more qualitative than quantitative to allow designers flexibility for creative solutions. Focus is on expressing a building's character through the key elements of that building type. One key element is the general form as expressed in the building's ground floor, middle, and top. Ground floor relationship to the street and the building's main entry is very important. Materials and details are also very important elements expressing a building's unique qualities.

### Marine Related Retail

This building typology includes retail related to the sale of boats and boat related products. Located both in the Craftsman District and the marina areas in the Water's Edge, these retail buildings will have a character that reflects the maritime environment. This character will be expressed through maritime inspired forms, materials, and details. Designers are encouraged to draw inspiration from sleek maritime forms and boat like nautical details like those found on well-designed sail boats.



Draw Inspiration from Nautical Details

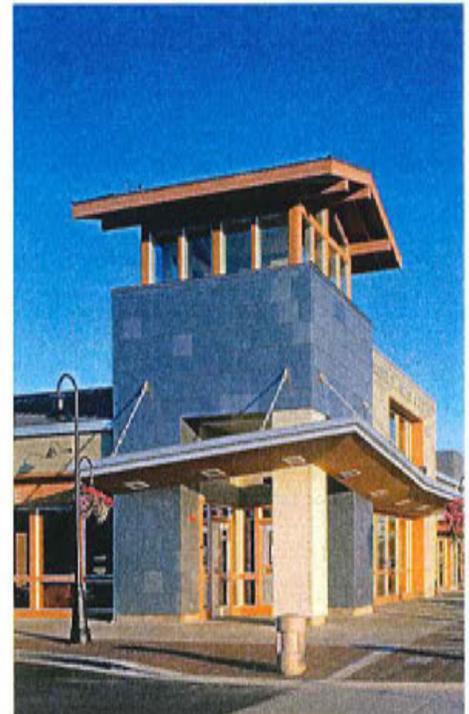


Marine Related Retail

Key design components include the following:

**Building Ground Floor** – Marine related retail should have clean visual lines and a strong presence at the ground floor that visually ties to the marina or to the working maritime businesses of the Craftsman District. Glass should be maximized to help create the visual tie. For boat sales, a strong connection should be made between the retail space and the boat display area. At the marina, retail buildings should be multisided and tie both to the boat moorage and Esplanade area as well as the upland area.

The ground floor design should appeal to the pedestrian through details and possibly with the integration of maritime based artwork. Entries should punctuate the façade with special details. Siding should emphasize horizontal lines, much like wooden boat hulls.



Marine Related Retail

Windows – Marine related retail can express its character through windows that use narrow dimensioned steel mullions and grid frames reminiscent of Deco or Bauhaus architecture, or perhaps cruise ships of that time period.

Building Top – Top treatments of marine retail buildings can be relatively simple without a lot of decorative detailing. They can have a distinct visual termination through a change of material, pattern, color, or form. Parapets should have a cap along the top that could be



Grid Frame Marine Retail Windows

done simply. Cornices, if used, may have some projection but need not be too fussy with decorative detail. An offset that captures a deep shadow line can create an appropriate terminus. Exposed pitched roofs, if used, can include eaves that could create a prominent edge when viewed from ground level against the sky.

Materials – Exteriors of marine retail should be constructed of materials that tie into a working maritime environment. Durability is expressed through the use of steel, heavy gauge aluminum or aluminum alloy, masonry, or wood that is treated to hold up in the marine environment. Masonry such as granite, ground face block, or a well-finished concrete could be the dominant ground floor material. Steel could be used for the roof structure and for window mullions. Pitched roofs should be a durable metal such as zinc or an alloy. The underside of overhanging roof eaves should be constructed of an expressive architectural material like high



Marine Durable High Grade Wood May Be Used

grade wood. In all cases, consider materials for their durability in the wet and windy marine climate.

Details – Relating to the boat aesthetic, details can express materials and their structural connections. Detailing should be revealed in marine retail buildings and should emphasize efficiency and logic in the connections between materials. They can be strong but elegant. Door hardware that has a sleek, nautical style and uses brass, stainless steel, or galvanized steel will help to express the marine aesthetic. Another source of inspiration may be masts and rigging that could be drawn upon for a structural reason and to create a special iconic characteristic that would make the building unique.

## Maritime Related Office

Office use related to maritime businesses may, in most cases, be consolidated within light industrial and retail commercial buildings. If independent office buildings are developed in the Craftsman District, they should follow these guidelines, but buildings with offices as a secondary use should follow the guidelines of predominant use. Located in the Craftsman District, these buildings will have office uses that will most likely be related to the maritime activities. As such, they should reflect the maritime character through maritime inspired forms and materials, and through nautical inspired details.



Maritime Related Office

This building typology includes the following key design components:

Ground Floor – All office main entries should be located on the ground floor and face the main street, access area, or prominent corner. Design should emphasize

the entry and use it to express the aesthetic quality of the Craftsman District through maritime inspired forms and materials, and sleek, boat-like nautical details such as door hardware and entry canopy connections. The entryway floor could be an opportunity for inlaid art or patterning that could express the nature of the building's business activities.



Entry Canopy with Nautical Inspired Light Fixture

**Windows** – Marine related office should use windows to create a strong rhythm that relates to the interior layout of offices. Since natural light is required in all offices, there should be a large quantity of windows. Ideally, many, if not all windows will be louvered or operable for workers to self control the heating and cooling of their space. Window character could draw on historic waterfront industrial buildings that used larger expanses of glazing with grid frames and narrow mullions.

**Building Top** – Top treatments of marine office buildings can be relatively simple, but should have a distinct visual termination through a change of material, pattern, color, or form. Parapets should be capped and cornices, if used, should project to capture a deep shadow line. The goal is to create a visually strong but elegant terminus that fits with the architectural design of the office building. Exposed pitched roofs, if used, can include eaves that should create a prominent edge when viewed from ground level against the sky.

**Materials** – Exteriors of marine office will be constructed of materials that tie office use with a working mari-



Office Building with Appropriate Materials and Window Layout

time environment. Materials should be both durable and professional to distinguish this building type from the others in the Craftsman District. Consider steel, heavy gauge aluminum or aluminum alloy, masonry, or treated wood. Masonry such as brick, granite, ground face block, or a well-finished concrete could be the dominant building face material. Pitched roofs should be similar to the other Craftsman District Buildings and be a durable metal such as zinc or an alloy. In all cases, consider materials for their marine climate durability.

**Details** – Details for the marine office buildings should relate to the maritime aesthetic, express materials and their connections, and also express the nature of the business activity. Because this is the Craftsman District, detailing that shows the connection of structure and materials should be revealed and well designed. Connections should be robust but elegant. Other detailing opportunities include door hardware and entry canopy design that draws on the maritime aesthetic and the vocabulary of nautical components such as masts, rigging, latches, round windows, and curved forms.



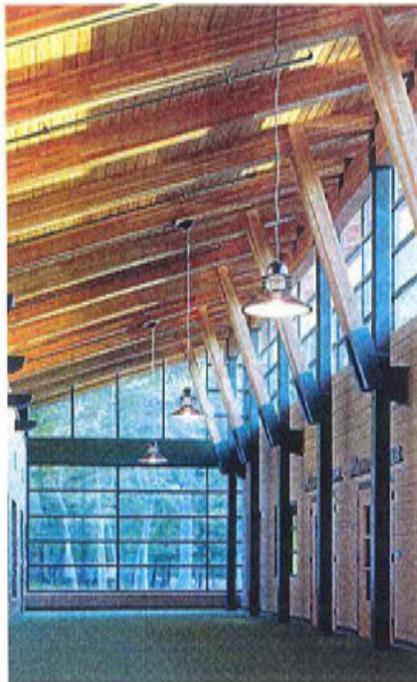
## Light Industrial Marine Services

This building typology includes light Industrial spaces related to boat repair and servicing. Located in the Craftsman District, these buildings will also reflect the maritime character, although they will be much more utilitarian in their structure, form, material, and detailing.

Key design components include:

**Ground Floor Design** – The maritime programmatic requirements of the ground floor will to a large extent dictate certain forms, materials,

and details of these buildings. As an example, boat repair and servicing space will be high bay to accommodate boats and will have large span spaces with large openings. These buildings will embody the craftsmanship component of the Craftsman District and should



Design For Craftsman Quality

architecturally express this on the ground floor. If the building includes a main entry, it should face the main street or access area and be used to express the building's use. Some buildings may include an office loft accessed from inside the main work space. This could also be expressed architecturally on the building facade.



Express Upstair Offices on Elevations

**Building Top** – The roofs of marine industrial buildings should explore using a monitor roof system similar to historic industrial and waterfront buildings. These provide light into the deep bays and can also provide ventilation.



Historic Wharf Buildings with Monitor Roofs

**Materials** – Exteriors of marine light industrial buildings will be constructed of materials durable enough for both their use and the climate. Materials should distinguish this building type from the others in the Craftsman District with more industrial steel (painted or galvanized), masonry ground face block, or concrete. If using monitor pitched roofs, they should be similar to the other Craftsman District Buildings and be a durable zinc or alloy.



Industrial Materials

**Details** – Details must express the craftsmanship of maritime industry through well designed structural connections such as turnbuckles and steel bolts. Where possible, consider revealing major structural connections and using them to express the industrial nature of the architecture. Unlike the other building types in this district, this building type is more robust and can use a less refined aesthetic of nautical based detailing.

### Marina Services

Marina service buildings will include bathroom and laundry facilities. These facilities will be housed in small one story buildings along the Water's Edge South Esplanade at the 14th Street Marina, and at the 12th Street Marina. These buildings will be strategically placed for convenient access to the docks and serve as a portal to the Esplanade.



Concept Design of a Marina Service Building

Their character should be expressed through maritime inspired forms, materials, and details related to the boating culture of which they are a part. Designers are encouraged to draw inspiration from maritime forms and boat like nautical details.

Key design components include the following:

**Ground Floor Design** – Being one story, the relationship between the ground floor and the marina should provide the strongest visual link. Ground floor design should be visually tied to the marina with maritime inspired forms, materials, and details. Visual interest should come through design details, materials, and color on both the parking side and the dock side. The

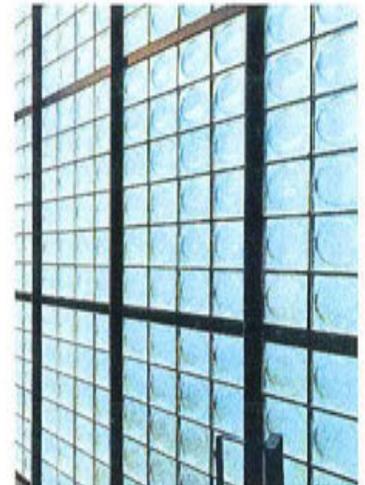


The Building May Serve as a Portal to the Marina

buildings, if varied, can serve to act as beacons to help guide incoming boats to their correct slips and docks. The building will be experienced on the pedestrian level and design should be appealing with rich ground plane details and possibly with the integration of maritime based artwork. Some of the buildings that act as pedestrian portals to the Esplanade should pronounce this connection by a canopy, lighting, or possibly a flag pole inspired from a sailboat mast. Siding could be designed to emphasize horizontal lines, much like wooden boat hulls.

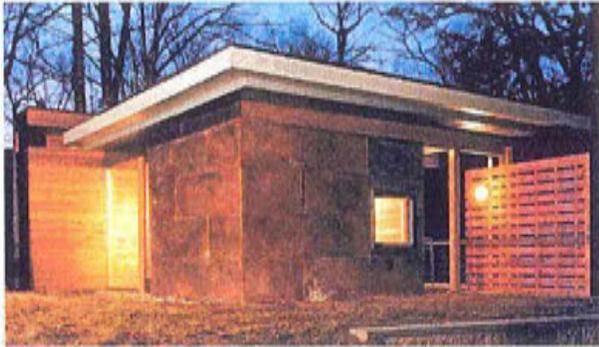
**Windows** – Programmatically, windows, if used in the shower and bathroom facilities, must have obscured glass. Back lit glass block, fritted, or colored translucent glass will

also act as a beacon at night. Windows for the laundry facilities or other uses should be ample to allow for views to and from the docks. These windows could also use the narrow dimensioned steel mullions and grid frames as suggested for the marina retail buildings. A round window that recalls a boat window may be an appropriate unique detail on some of the buildings.



Restroom Obscured Glass

**Building Top** – Top treatments of marine service buildings can be relatively simple without a lot of decorative detailing. They can have a distinct visual termination through a change of material, pattern, color, or form. Parapets should have a cap along the top that could be done simply with steel. Cornices, if used, may have some projection but need not be too fussy with decorative detail. An offset that captures a deep shadow line can create a strong but elegant terminus. Exposed pitched roofs, if used, can include eaves that could create a prominent edge.

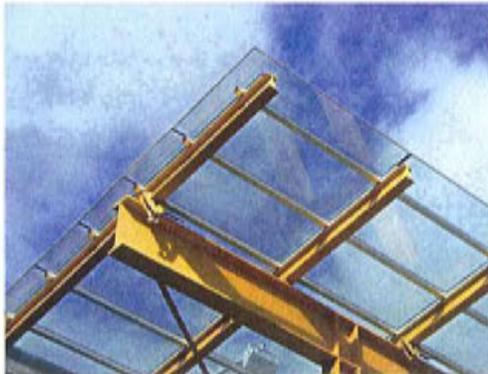


Design as Visual Beacons

**Materials** – Exteriors of the marine services buildings should fit with the marina environment. Like the surrounding marina, the use of steel, heavy gauge aluminum or aluminum alloy, masonry, or treated wood could all be appropriate. Masonry such as granite, ground face block, or a well-finished concrete could be the dominant base material with steel used for the roof structure and for window mullions. Pitched roofs should be a durable metal or a high strength glazing.

**Details**– Details should be used to help reinforce the connection between the building and the maritime aesthetic. Since the buildings are small, a few well thought through

and well designed connection details between materials could help provide pedestrian interest and reveal an aesthetic connection



Details and Materials for Marina Service Buildings

to the marina. Other details such as door hardware could be of a sleek, nautical style and uses brass, stainless steel, or galvanized steel. As with the marina retail buildings, another source of inspiration may be masts and rigging that could be drawn upon to create a special iconic characteristic or mark the portal between the parking lot and the boat moorage.

## Commercial Retail

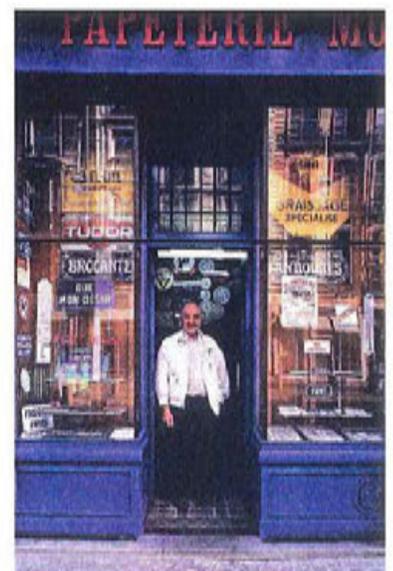
Commercial retail includes buildings that will house retail related to the sale of a wide variety of goods, merchandise, and services. Located predominantly in the Captain's Walk/ 12th Street Retail District, these retail buildings will create the focal point of the community and create a place imbued with a unique maritime village character. This will be achieved in part by these standards and by the use of historically reflective and maritime inspired forms, details, and materials. Some retailers may also locate near the main intersections in the residential neighborhood.



Captain's Walk Design Should Draw From Historic and Maritime Qualities

Key design components of the commercial retail building type include:

**Ground Floor Design** – Retail relationship between the ground floor and the street shall provide for maximum visibility through storefront glazing, and maximum visual interest through design details, color, and decoration. Buildings should incorporate historical maritime and nautical inspired details, forms, and materials to rein-



Maximize Storefront Visual Interest



Create Storefront Variety Along the Street

force the aesthetic quality of the Captain's Walk. Large storefront windows should be articulated with decorative trim and sill details. "Clerestory" windows located above the principal storefront windows are encouraged. Individual storefronts must provide elements of uniqueness to add to the diversity and visual interest of the street. These buildings are encouraged to more traditional than modern and should include revealed bays, overhangs, art, hanging or mounted signage, decorative inlays, masonry strips, and/or cornice lines.

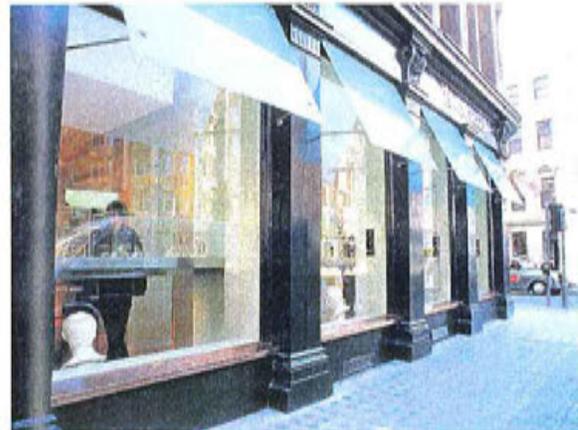
Decorative canopies or awnings should be included at the entry and along storefronts for both weather protection and visual interest. They will serve to emphasize entries and create a pedestrian scale at the retail street level. They can also provide sheltered areas for market stands to engage the street.



Include Storefront Awnings and Canopies



Building Top – Top treatments of retail buildings should include decorative detailing and forms. They should achieve a distinct visual termination through a change of material, pattern, color, and form. Parapets should have a larger cap on top, and cornices, if used, should

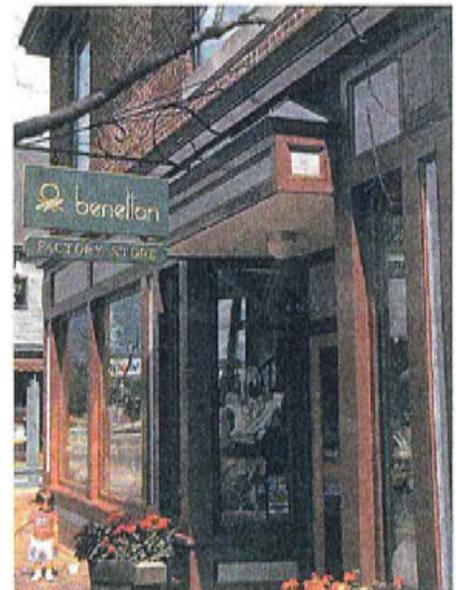


Projected Cornice Creates Shadow Line

have projection with decorative brackets and detail. Molded forms should cast shadows to create a visually rich and elegant terminus. Exposed pitched roofs, if used, can include eaves and perhaps exposed roof joists or beams.

Materials – Exteriors of the retail should be constructed of materials that reflect a historic and maritime environment and help to create variety and visual interest. Traditional materials such as brick, granite, and protected wood should be used. Limited use of steel and other metals may also be acceptable. Storefront windows should be comprised of clear, "vision" glass. For pitched roofs, slate, metal, or a composite mate-

rial should be used. Storefront windows should be comprised of clear, "vision" glass. For pitched roofs, slate, metal, or a composite material should be used.



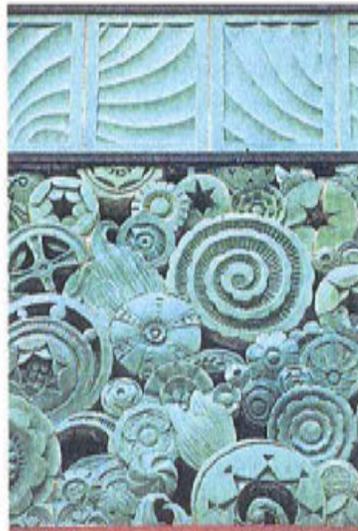
Use Traditional Materials



Inspiring Retail Space, Materials, and Decorative Details

rial may be used. Color, texture, and unit size of composite roofs must have an appropriate to the district's character. If there are pitched roofs, the underside of overhanging roof eaves should use a high grade wood or a decorative material. In all cases, consider materials for their durability in the wet and windy marine climate.

Details – Details of the commercial retail buildings should be inspired by historical maritime and nautical elements to reinforce the aesthetic quality of the Captain's Walk. Relating to this aesthetic, details can express materials and their connections by being revealed and emphasized. In the Captain's Walk, details should be more decorative, traditional, and varied than in other districts. Include decorative paneling and inlays, or well designed functional elements. Door hardware provides one opportunity for referencing a nautical style with brass maritime forms. Window trim and mullions can help to express a historic marine architecture. Another source of inspiration may be masts and rigging that could be drawn upon to create a special character for canopies and signage.



Use Decorative Details on Storefronts

## Commercial Office

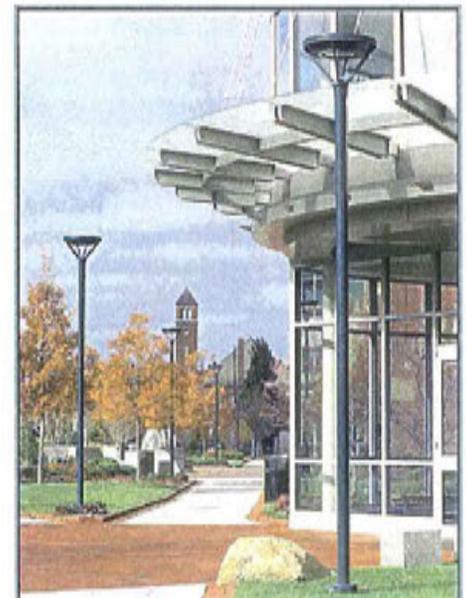
Offices may be predominantly consolidated in the Gateway District, although other buildings in other districts will include some office space. Buildings that emphasize the predominant use as office should adhere to these guidelines. Being planned mostly for the Gateway District, these buildings will reflect that District's character with forms and materials that project a professional image while at the same time tie into the community wide maritime heritage and quality of Port Gardner Wharf.



Commercial Office Buildings

This building typology includes the following key design components:

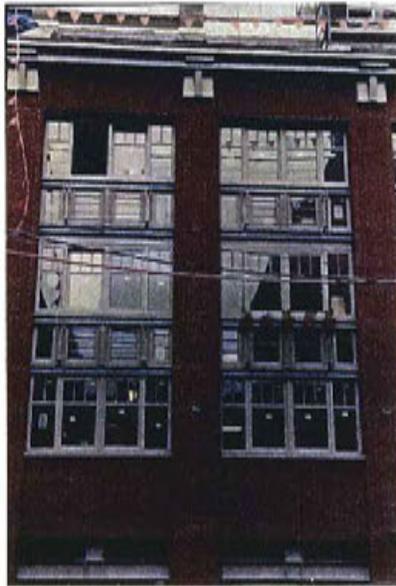
Ground Floor Design – Office relationship between the ground floor and the street shall provide for visual interest with special forms, materials, and details to tie into the District. Locate office building main entries on the ground floor facing the main street or the most prominent corner. Main entrances to offices must present a strong presence. Architectural design should emphasize the entry and use it to express the aesthetic quality of the district. Entry details such as door hardware,



Office Main Entry Must be Prominent

entryway floor, and entry canopy connections provide opportunities to give a unique signature to the building and can express the nature of the building's business activities.

**Building Middle—** Office space may include a second and in some cases a third story. Exterior elevation facades of the building should reveal the repetition and rhythms of office spaces through window treatments and materials. Proportions and patterns of windows must be well designed and organized to create a strong rhythm that is visually pleasing



Office Elevation Proportion and Pattern

from the exterior, and also provides natural light to all offices. Ideally, many, if not all windows will be operable for workers to self control the heating and cooling of their space. South facing windows should be louvered for shade. Window character should draw on and blend the qualities of professionalism and maritime character.



South Facing Window Louvers

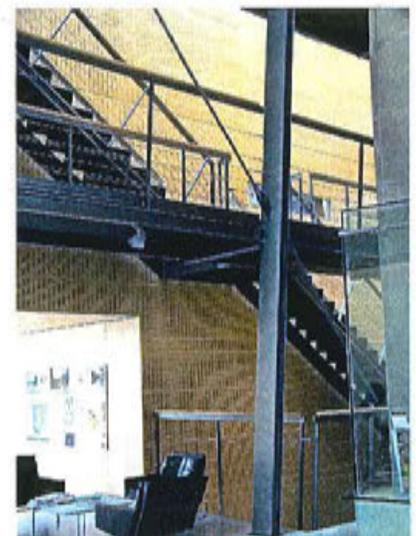
**Building Top –** Top treatments of commercial office buildings need not be overly ornate, but should have a distinct visual termination through a change of material, pattern, color, or form. Parapets should be capped and cornices, if used, should project enough to produce a shadow line in order to create a visually strong but elegant terminus that fits with the architectural design. Exposed pitched roofs, if used, should include prominent eaves.



Projected Eave Roofline

**Materials –** Exteriors of commercial office buildings will be constructed of materials that are durable, and evoke a professional image that is distinguished from the other districts. Masonry such as brick, granite, or a well-finished concrete could be the dominant building base material. Masonry cladding or finished steel can express the rhythm of the underlying structural system. Pitched roofs, if used, should be similar to the other district buildings and be a non-corrosive metal. Hardware should be metal and window frames should be steel, aluminum, or wood clad.

**Details –** Details for commercial office buildings should relate to the district aesthetic, and reveal the inherent quality of materials. They can help to express the nature or professionalism of the



Office Details and Materials

business activity by being well designed. Structural connections can be revealed and expressed in a straight forward manner that is clear and strongly articulated. Other detailing opportunities include door hardware and entry canopy design that is inspired from a maritime aesthetic.



Waterfront Hotel Will Have a Grand Entry

## Hotel

A waterfront hotel may be developed in the West End District. The hotel can serve as a destination and anchor for the west edge of the wharf. Its character should emphasize hospitality and comfort with the physical design being evocative of a maritime heritage. The hotel should be high quality in its form, materials, and detailing to further enhance the Port Gardner Wharf character.

The hotel should include the following key design components:

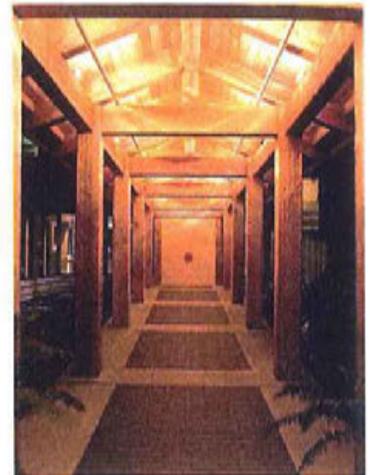
**Ground Floor Design** – The hotel's relationship between the ground floor, street, water, and Esplanade is very important. Arrival and entry spaces shall provide maximum visual interest with special and elegant forms, materials, and details. A portico, main entry, and lobby should present a strong presence and be the public face that connects the main street with the hotel's prominent gathering space. It should tie to the Esplanade and views to the waterfront. Architectural design should

emphasize the entry and use it to express the aesthetic quality of the hotel. Entry details on the door, entryway and lobby floor, and entry structure can foster a

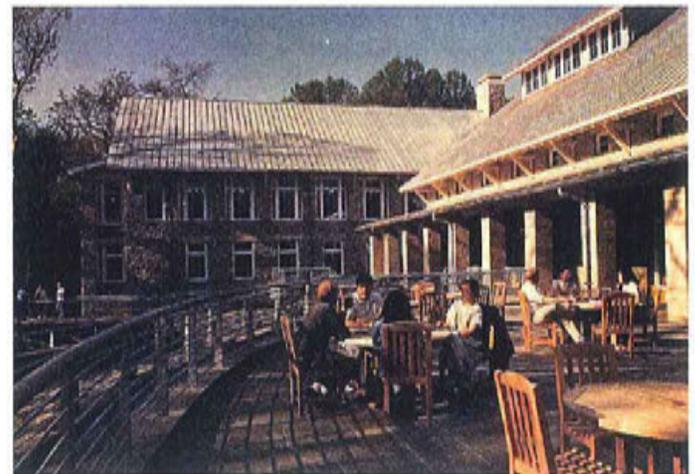
unique identity to the building.

**Building Middle** – Hotel room layout

will organize exterior elevations and reveal repetition and rhythms. These should be well designed and articulated through high quality decorative window treatments and materials. Proportions and patterns of windows should be organized to create a strong rhythm that is visually pleasing from the exterior. Other areas such as amenity spaces like terraces must be integrated with the overall design.



Hospitality Expressed through Design



Hotel Amenity Spaces such as Terraces Must be Well Integrated

**Building Top** – Top treatments of the hotel should have a distinct visual termination through a change of material, pattern, color, or form. The roof terminus should fit with the architectural design character. Parapets should be capped and cornices should have a strong visual

projection. Exposed pitched roofs, if used, should include prominent eaves. Roof design must be cognisant of views from nearby residences and therefore must be visually pleasant and unobtrusive.



Waterfront Hotel

**Materials** – Hotel exteriors must use durable, but welcoming materials. Materials will help evoke a sense of comfort and hospitality. Masonry such as brick or granite, or wood could be the dominant building material. Masonry cladding or a stucco finish may be used in some areas. The rhythm of the hotel windows can be separated by material that could be made expressive through patterning or detailing. Pitched roofs, if used, may be similar to the other area buildings and be a non-corrosive metal or they could be done with a composite material that has a similar look of wood shingles.

**Details** – Details for the hotel can be decorative to help create a distinct look and quality. They can work to add scale and visual interest in the public areas for guests and visitors. Detailing should be inspired from a maritime aesthetic and well designed to express the quality and character of the hotel. The hotel could use trim, molding, decorative hardware, and articulation of details throughout.

## Restaurant

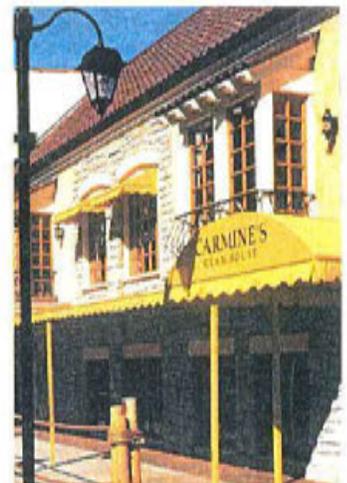
Restaurants will be predominantly connected to the waterfront in the West End District and along the Water's Edge. They will serve as a destination and part of the attractive quality at Port Gardner Wharf. Design should emphasize their connection to the water and to the streetscape. Restaurant character should emphasize comfort and ambiance through evocative design that draws on the maritime heritage. Restaurants may be varied in their cuisine and character, but all should be of a high quality in their form, materials, and detailing.



Connect Waterfront Restaurants to the Water

Restaurants should include the following key design components:

**Ground Floor Design** – The restaurant's relationship between both the ground floor entry and the water is very important. The entry is the public expression of the restaurant and it should connect strongly with the main street, and be warm and inviting. There should be ample visual character at the entry with special forms, materials, and details. Views from the outside to the inside should be enticing



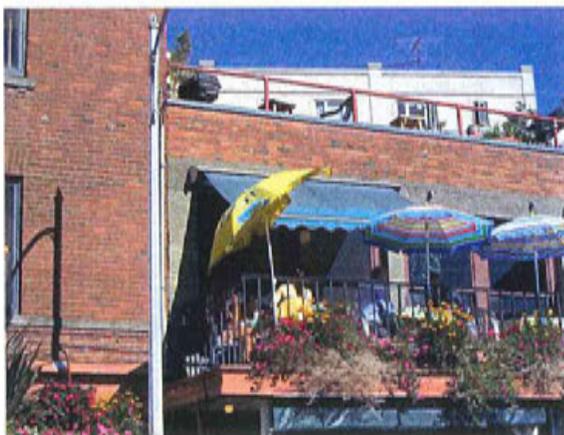
Create an Entry Expression



Provide Areas for Outdoor Seating

and views from tables to the water should be maximized. Restaurant program and table layout should respond to the view orientation. It will determine the basic configuration of the restaurant. Some waterfront restaurants use a split level design to maximize view tables and separate lounges and banquet rooms. Restaurants bordering the Esplanade should be connected and provide an area for outside seating. Service entries and mechanical equipment must be out of view.

**Building Top** – Restaurant roofs should fit with the architectural design character whether using capped parapets, cornices, or pitched roofs with prominent eaves. Roof design must be cognasent of views from nearby residences and must be visually attractive.



Restaurants May Include 2nd Story Seating

**Materials** – Restaurant exteriors should use warm and welcoming materials. Masonry such as brick or granite, or wood should be the dominant building material. Windows should be maximized to connect the interior and exterior with views both in and out. Pitched roofs, if used, could use slate, wood shingles, or a composite material that has a similar look of wood shingles.

**Details** – Details for the restaurant should be decorative to help create the restaurant's distinct look and quality. The details will add scale and visual interest and if appropriate to the restaurant's cuisine, they should be inspired from a maritime aesthetic. In keeping with the goal of creating a high quality place, details such as trim, molding, decorative hardware, and articulation throughout the restaurants should be well designed.



Simple and Effective Cornices, Materials, and Detailing



Mixed Use Creates Livelier Streets

### Mixed Use

Mixed Use includes buildings that will have a mix of residential with retail or office uses. Located predominantly in the Captain's Walk/ 12th Street Retail District, these buildings will activate the street with residents. Having residents will extend the times of lively active streets and help create a stronger sense of community. Being primarily in the Captain's Walk District, the historic maritime character should be expressed through the use of maritime forms, materials, and details.

These are as described in the following key design components:

**Ground Floor Design** – For most mixed use buildings, the ground floor will be retail and its design should follow the guidelines for retail. The upper floors of residential will provide a main entry on the street which should create its one visual interest through design details, color, and decoration.

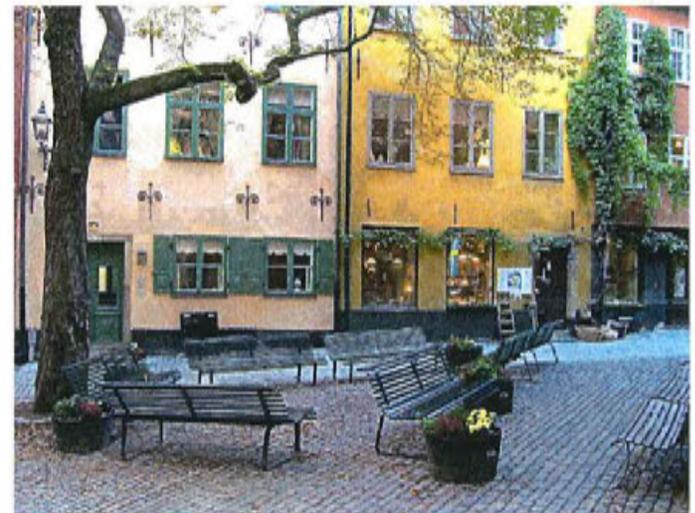
**Entries and Doors** – Materials, forms, and details inspired from historic nautical elements will reinforce the

aesthetic quality of the Captain's Walk at the entry. The entry should include an overhang, and possibly artwork, hanging or mounted signage, decorative inlays, masonry strips, cornice lines, and other decorative detailing and articulation. Canopies, awnings, and overhanging balconies or bays provide both weather protection and visual interest which will further serve to emphasize entries and create a pedestrian scale at the retail street level.



Active Ground Floor with Overhang

**Building Middle**  
– Residential unit

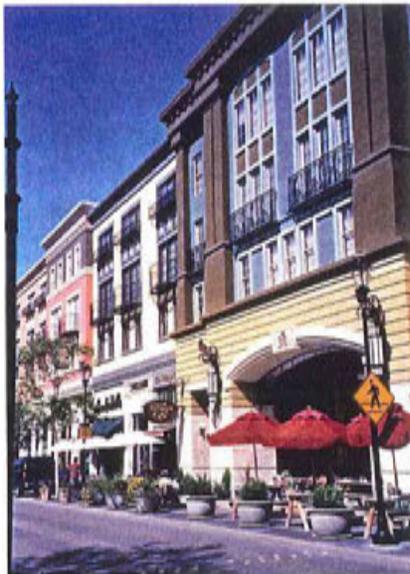


Pedestrian Scale Ground Floor

layout on the upper floors will help organize exterior elevations and reveal repetition and rhythms with windows and balconies. These should be well designed and articulated through high quality decorative window treatments and materials. Proportions and patterns of windows should be organized to create a strong rhythm and balance to the exterior elevations.

**Building Top** – Top treatments of the mixed use build-

ings should have a distinct visual termination through a change of material, pattern, color, or form. Parapets should be capped and cornices, if used, should project in order to create a visually strong but elegant terminus that fits with the architectural design. Exposed pitched roofs, if used, should include prominent eaves.



Mixed Use Ground, Middle, and Top

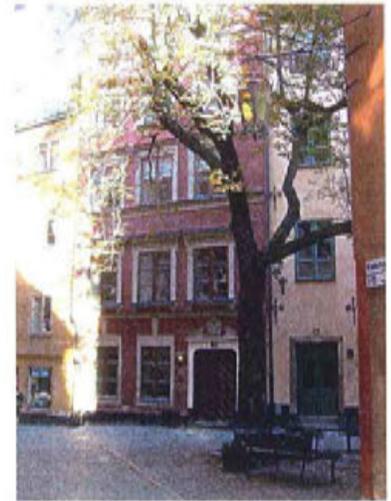
Materials – Exterior materials on the ground floor will convey the unique district identity with granite and/or brick. Limited use of steel may also be acceptable. For pitched roofs, slate, metal, or a composite material may be

used if color, texture, and unit size have an appropriate historic or natural look. Hardware could be brass or steel and the entry way ground plane surface should include stone or tile paving. If concrete is used, it should have an inlaid ornamental building name or address.

Details – Details of the mixed use buildings should be inspired by historical maritime and nautical elements to reinforce the aesthetic quality of the Captain's Walk. Relating to this aesthetic, details that are revealed and emphasized can express materials and their connections. They should be more decorative, traditional, and varied than in other districts.

## Residential Flats

This building typology is one of the most prominent residential building types at Port Gardner Wharf. A wide range of flats from studios to multi-bedroom and penthouses will be located in the buildings that make up the residential neighborhood District. These buildings will be a dominant element in creating the character

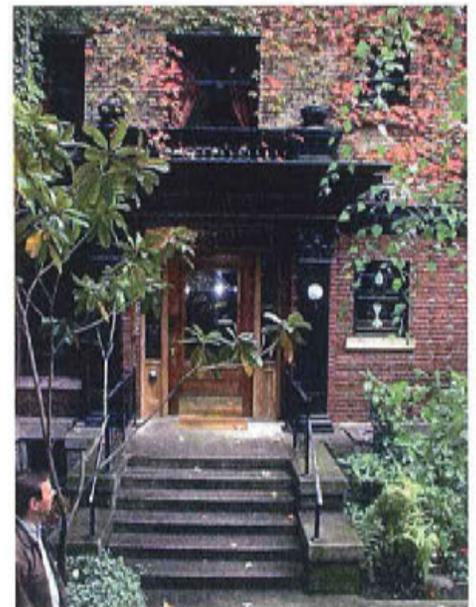


Residential Flats

of the residential neighborhood. A rich palette of forms, materials, detail and décor will work toward developing the neighborhood's timeless and comfortable character.

Some key design components for this building type include:

Ground Floor Design – Ground floor of the residential flat buildings should convey the District's character as a comfortable and inviting place to live. Design details, lighting, a canopy, and decorative elements should emphasize the entries, provide visual interest, and create a strong relationship between the ground floor and the street. Public space and private space should be clearly defined with well designed entrances making the buildings welcoming at the street level.



Comfortable and Elegant Residential Entry

Ground floor windows along pedestrian corridors shall balance visibility from residences with privacy. Windows must be articulated with trim or masonry frame borders and sills. Individual buildings must provide some elements of uniqueness from neighbors to add to the diversity of the street. Ground floor design is also encouraged to include bays, overhangs, art, decorative inlays, masonry strips, and/or cornice lines. Entries for structured parking should not interfere with the main pedestrian entry. A clearly visible building name and address should be included near the main entry.



Historic Brownstones Convey the Desired Qualities for Residential

**Building Middle** – Residential buildings with stacked flats should be well designed with repeating window patterns, a change in colors and materials, and provision of second story balconies, dormers, or bay windows on all street and major courtyard elevations. A change in the building plane may also be considered to reduce bulk on large buildings. Unit layout on the upper floors should be reflected in exterior elevations by revealing repetition and rhythms with windows and balconies. Proportions and patterns of windows should be organized to create a strong rhythm and balance to the exterior elevations. Upper floors should use large or bay windows to maximize views.

**Building Top** – Top floors and penthouses should express a change of character through increased glazing area, larger decks, and possibly by stepping back. Roofline treatments should have a distinct visual termination. Exposed pitched roofs shall be no less than 4:12, and should include prominent eaves. If not pitched, they shall be stepped and terraced. Cornices, if used, should project in order to create a visually strong but elegant terminus that fits with the architectural design. There should be a change of material, pattern, color, or form.



Residential Flat Building Top with Pronounced Eave

**Exterior Materials** – Residential exterior materials will help convey the unique neighborhood identity of the residential district by being high quality and visually interesting. Classic masonry materials such as granite and/or brick should be used. Concrete may be considered with acceptable finishes or cladding. Well treated wood and a limited use of steel and may also be acceptable. For pitched roofs, slate, metal, or a composite material may be used if color, texture, and unit size have an appropriate classic or natural look. Hardware could be brass or steel. Entry way ground plane surface should include stone, tile, or concrete with an inlaid ornamental building name or address.

**Details** – In the neighborhood residential district, details may be more decorative and evoke a more historic, traditional, and varied character than in other districts. They should add to the timeless and comforting quality conceived for the neighborhood.



Townhouses Create a Strong Neighborhood Character

## Townhouses

The Townhouse typology provides a multilevel housing choice on the minor residential streets. Located exclusively in the neighborhood residential district, townhouses will create a smaller scale character. With individual entrances on the street, they create a distinct neighborhood feel and can connect well to the street.

**Ground Floor Design** – Townhouses should create a strong relationship between the main floor and the street by providing stoops or entry courtyards with well articulated entrances. Entries will help to refine the District’s character as a comfortable, elegant, and timeless place to live. They should provide visual inter-



Townhouse Stoops Connect to the Streetscape

est through design character, lighting, and decorative detailing. Individual townhouses should provide some elements of uniqueness from its neighbors to add to the diversity of the street. Entries should also display an ornamental townhouse address. Ground floor windows along pedestrian corridors shall balance visibility from residences with privacy. Garage entrances should be alley access where possible.

**Building Middle** – Each townhouse unit second floor elevation should also have some distinct qualities of variation that help to add interest within the rhythm of windows on units. They should include balconies, terraces, dormers, or bay windows, as well as a rich palette of colors and materials. A change in the building plane could be considered between town homes. Upper floors could use large or bay windows to maximize views.

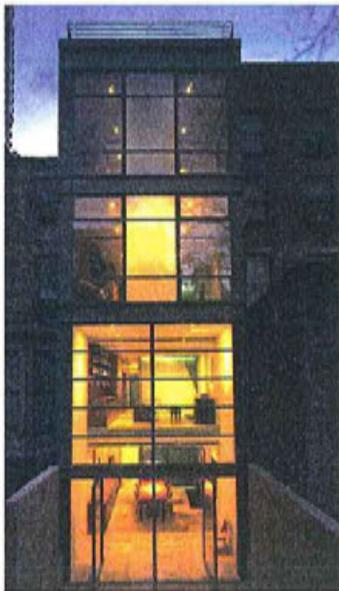


Bay Windows Maximize Views

**Building Top** – Building roofs should help to convey the distinct difference between homes. Pitched roofs should be no less than 4:12 and should include prominent eaves. If not pitched, they should be stepped and terraced. Flat roof parapets should terminate with a cap.

**Materials** – High quality and visually interesting masonry and wood (marine durable) will help to convey the qualities of the neighborhood residential district. Ground floor masonry should be granite or brick (cladding is acceptable). For pitched roofs, slate, metal, or a composite material may be used. Color, texture, and unit size of composite roofs must have an appropriate natural look. Entry ways should use the best materials with ground plane surfaces done in stone, tile, or integral colored concrete.

Details – Townhouse details should convey the image of being well crafted and permanent with a timeless and comforting quality. The residential scale can lend itself to being more decorative and traditional, but with some unique and varied character between units. Interesting details focused at entries can give each townhome its own identity.



Less Traditional Residential Lofts



Timeless Quality in Details



Balconies Add Spatial Definition to Lofts

## Lofts

Lofts are a unit type that may be mixed into larger buildings that contain flats, but if used predominantly in a building, may create its own building type and character. Loft buildings may create a smaller scale neighborhood character than stacked flats and should be located on minor residential streets on the edges of the residential neighborhood district. They provide a transitional building type that can be less traditional in style and fit better with streets that may also include commercial uses. With their two level layout and potential for individual entrances on the street, they can and should create a strong streetscape connection.

Lofts should consider the following key design components:

**Ground Floor Design** – Freestanding lofts can create a strong relationship between the main floor and the street by providing entry courtyards with well articulated entrances. Loft entries should provide visual interest through design character, lighting, and decorative detailing. Individual loft units should provide some elements of uniqueness to add to street façade diversity. Ground floor windows along pedestrian corridors shall balance visibility from residences with privacy.

**Building Middle** – Each loft unit second floor elevations should also include some variation to add interest within the rhythm of windows on units. Balconies and terraces and perhaps a change in the building plane should be considered between lofts to provide distinctness and spatial definition. Upper floors should use large windows to maximize views.

**Building Top** – Building tops on lofts should be stepped and terraced. Flat roof parapets should terminate with a cap or reveal and perhaps a change of material, pattern, color, or form.

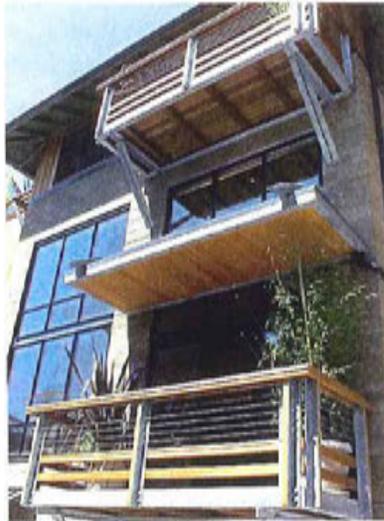
**Materials** – Loft materials may be less richly decorative than other residential housing types, but quality should still be high and visually interesting. In addition

to granite or brick, masonry may be well finished concrete. Marine durable wood could also be used to help convey a warmer quality for the neighborhood. Metal or a composite roofing material with an appropriate look may be used. Entry ways should use the best materials with ground plane surfaces done in stone, tile, or integral colored concrete. Entry materials can be varied at the different loft unit to give each its own character.

**Details** – Detailing on loft buildings should convey a high level of craftsmanship. Loft details can help merge the residential scale and quality with other building types and can be less decorative and traditional. De-



tails should help to provide some unique and varied character between units with the most interesting details being concentrated at entries and other pedestrian areas.



Material and Detail for Lofts

## Live/ Work Residential

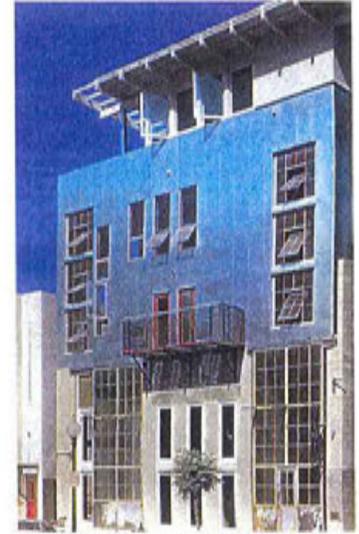
Live/ Work residential buildings will add an interesting building type that provides housing and work space for artists, designers, and other sole proprietors. Located potentially in the Captain's Walk/ 12th Street Retail Area, or possibly at the edge of the residential neighborhood district, these buildings will activate the street and create a more lively and interesting character. Like lofts, they provide a transitional building type that can be less traditional in style and fit better with streets that may also include commercial uses.



Live/ Work Units Can Engage the Street with Work Space Frontage

The key design components include:

**Ground Floor Design –** Live/ Work buildings must create a strong relationship between the ground floor and the street by providing work spaces and entrances with ample visual interest. Individual live/ work units should provide some unique elements and variation to differentiate each from its neighbors and add architectural diversity on the street. Main entrances should be large, recessed, or covered, and made



Live/ Work Ground, Middle, and Top

welcoming by emphasizing details and lighting. Incorporate the most expressive materials and decorative detailing near the entry, including special ground plane surface treatments. Ground floor windows along pedestrian corridors shall have partial visibility to and from work spaces with some opportunity for flexibility between visibility and privacy.



Live/ Work Ground Floor Work Space

**Building Middle –** Each live/ work unit should also include some variation on the second floor elevations. Balconies and terraces will provide distinctness and spatial definition within the rhythm of unit windows. Upper floors should be located and sized to maximize views.

**Building Top** – Building tops on live/ work units should be stepped and terraced. Flat roof parapets or cornices should terminate with a cap or reveal and perhaps a change of material, pattern, color, or form. Exposed sloped roof eaves must be prominent. Brackets supporting an overhanging roof should be expressive architectural features. Roof forms, if pitched, shall be no more the then 6:12.



Expressive Live/ Work Roof

**Materials** – Live/ work building exteriors should use materials that are less decorative and more utilitarian than other residential housing types. Quality should still be high and visually interesting. Masonry could be granite, brick, ground face block, or well finished concrete. Marine durable wood could also be used to convey a warmer quality, or steel to tie in with the work aspects of a live/ work typology. Metal roofing with an appropriate color may be used. Entry ways can be varied but should



Live/ Work Materials and Details

have ground plane surfaces done in stone, tile, or integral colored concrete.

**Details** – Detailing live/ work buildings should convey a high level of craftsmanship to merge a residential scale and quality with a commercial working building. Details can be less decorative and traditional, but should still help to provide some unique and varied character between units. The most interesting details should be concentrated at entries and other pedestrian areas.



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