

CHAPTER 2 SITE/PROPOSAL DESCRIPTION AND ALTERNATIVES

2.1 DESCRIPTION OF THE PROJECT SITE AND DEVELOPMENT HISTORY BY PARCEL

The project site is located on the west side of the Snohomish River, east of I-5, south of Pacific Avenue, and north of Rotary Park and Lowell Snohomish River Road (see Figure 2.1-1, Vicinity Map and Figure 2.1-2, Site Boundary/Layout, and Figure 2.1-3 Existing Site Topography). The proposed redevelopment area entails approximately 221 acres. The geographic scope of the project site is broadly defined such that it includes properties in the description that are not presently controlled by OliverMcMillan LLC (the Proponent) but may be added later and/or will be included in the Rezone and PDO actions, so the impacts of adding those areas is considered in the scope of analysis.

For purposes of describing portions of the site, this analysis will refer to six distinct geographic areas as described below (See Figure 2.1-2).

2.1.1 Landfill/Tire Fire Site

This area is bordered on the north by 36th Street; on the west by the Burlington Northern Santa Fe (BNSF) mainline and “Turkey Trail” rights-of-way; and on the east and south by the diagonal created by the former Milwaukee Road railroad right-of-way.

This portion of the site was initially used as a landfill and railroad operations starting in 1917 (see Figures 2.1-4 through 2.1-12, Historical Aerial Photographs), with landfill operations eventually encompassing the whole area which continued until 1974 when it was capped and closed. In 1977, a tire recycling operation began on site, was abandoned and later caught fire on two separate occasions in 1983 and 1984. Snohomish County constructed and operated a solid waste transfer station on the site from 1974 until it was closed, demolished and the site area graded by 2003. In general, the site is covered with fill materials from different origins placed to level the site or as a cap to the historical landfill activities. In 1974, 50,000 cy of soil was stockpiled and eventually covered the former landfill area. In 1995, approximately 226,000 cy of fill was placed over the landfill area. Other fill events likely occurred but were not documented in the existing literature.

Ownership of this area was split between railroads and the City for nearly a century. The southern triangle of the area and roughly the west third of the remainder of the property was owned by railroads and their affiliates including the Milwaukee Chicago and St. Paul, Great Northern, Northern Pacific, Burlington Northern and Glacier Park Company until 1992 when it was purchased by the City. Also, railroad lines that have been owned and operated by the BNSF (former Milwaukee Road railroad right-of-way) run through project area turning northeast from the mainline (splitting the Simpson properties and associated wetlands from the landfill parcel) turns north (separating the landfill from the Snohomish River), bisects the Eclipse parcel and re-connects with a branch of the mainline north of Pacific Avenue. This railroad line is being relocated as part of another project and most of the property associated with it will be incorporated into this Project. Table 2.1-1 summarizes the historical land use.

Map Revised: 12/12/07

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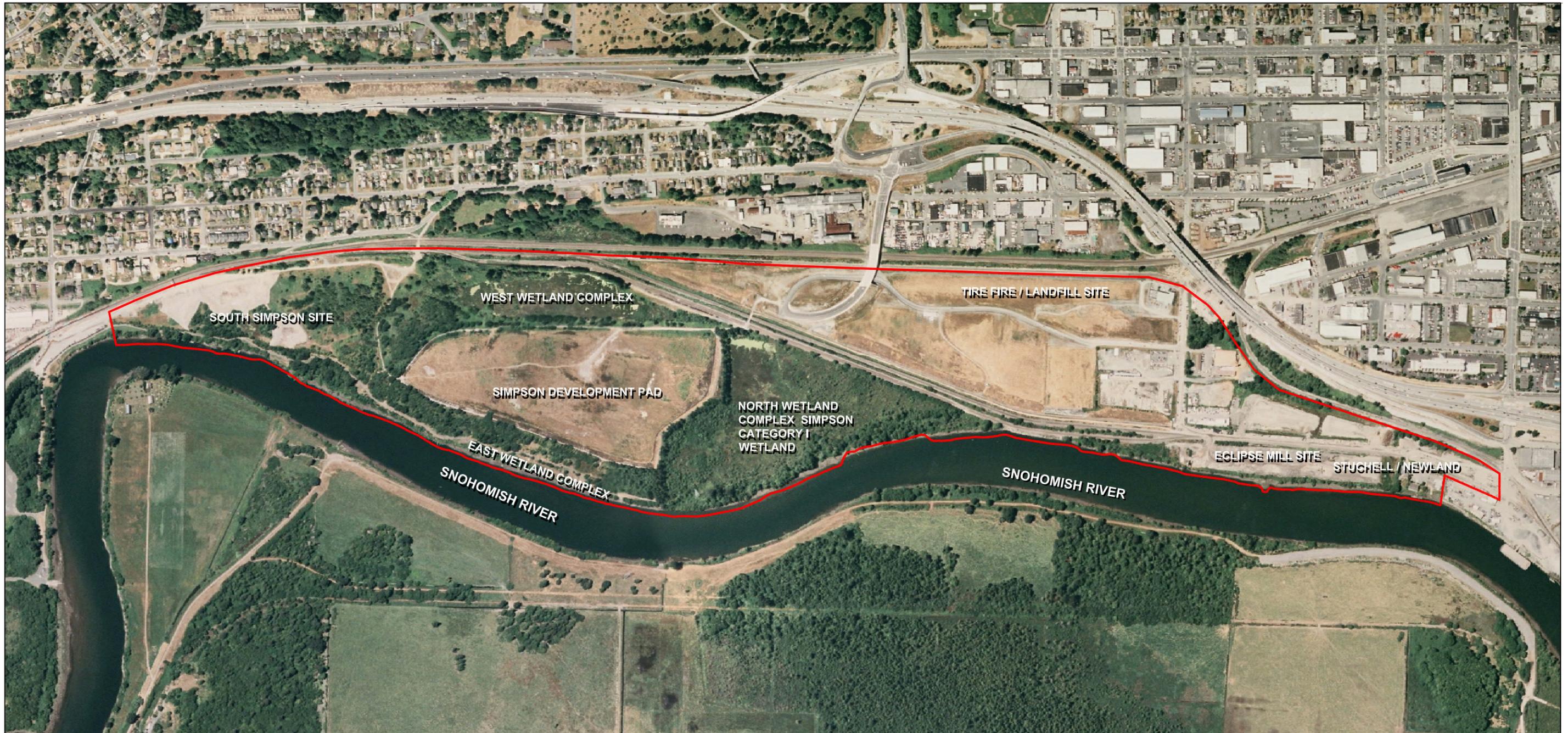
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Data Sources: ESRI Data & Maps, Street Maps 2005
 Transverse Mercator, Zone 10 N North, North American Datum 1983
 North arrow oriented to grid north

Vicinity Map

Everett Riverfront Redevelopment
 Everett, Washington

Figure 2.1-1



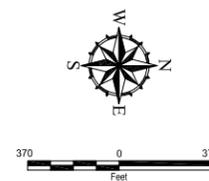
Legend

 Site Boundary

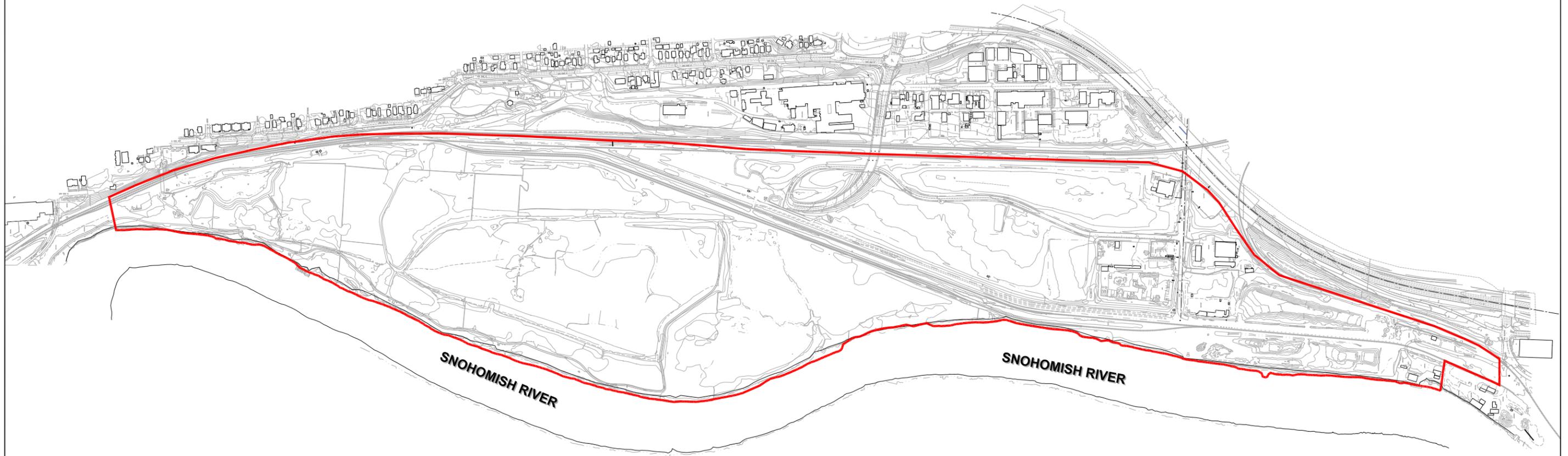
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Data Sources:
 Aerial photo obtained from NAIP Imagery 2006.



Site Boundary / Layout	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS 	Figure 2.1-2



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Reference: Drawing provided by Perteet, Inc.

Legend

 Site Boundary

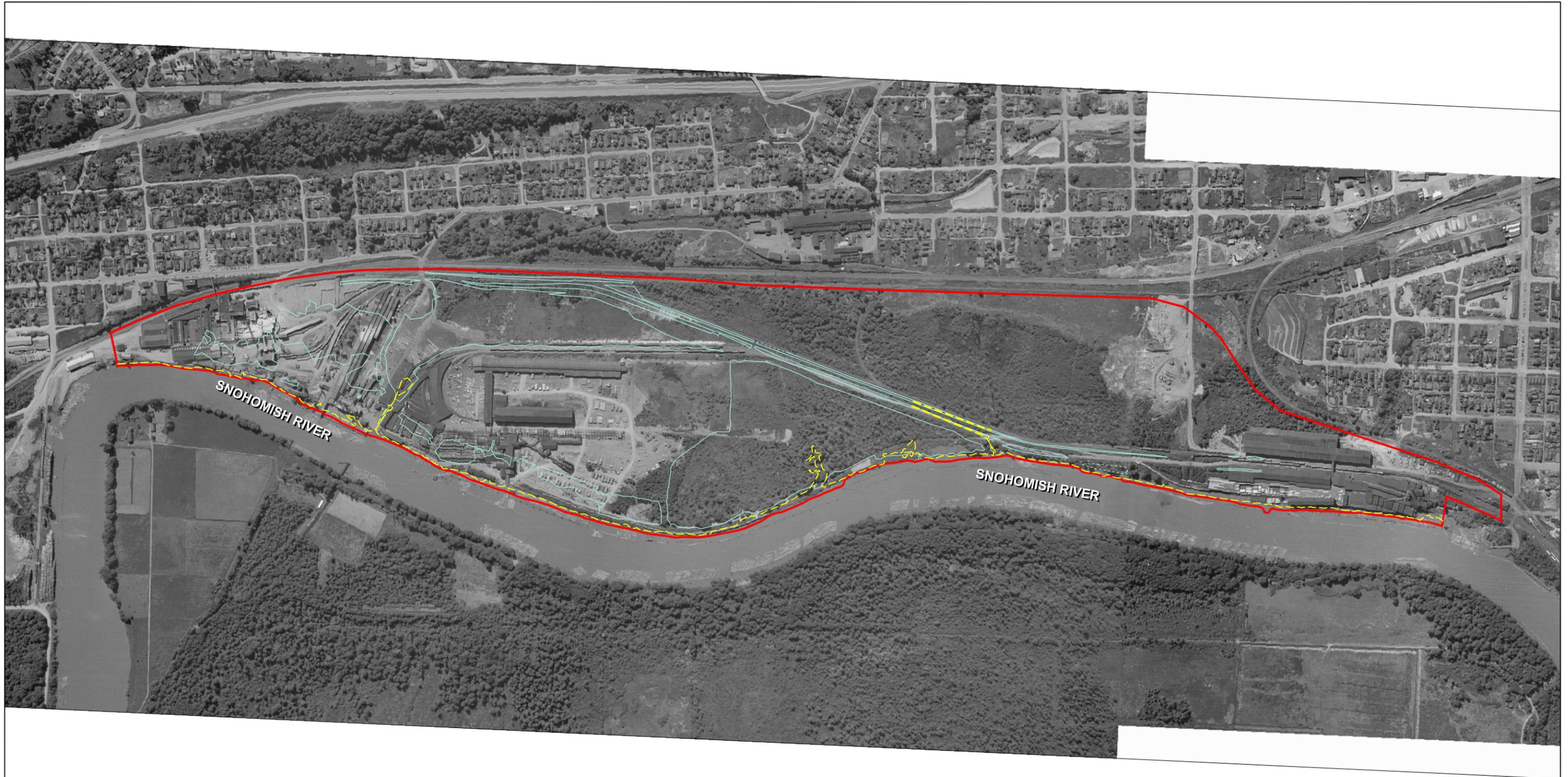


Existing Site Topography Overall

Everett Riverfront Redevelopment
Everett, Washington



**Figure
2.1-3**



Notes:

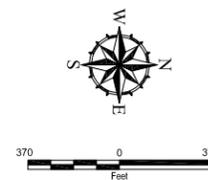
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4. Shoreline jurisdiction was provided by Perteeet.

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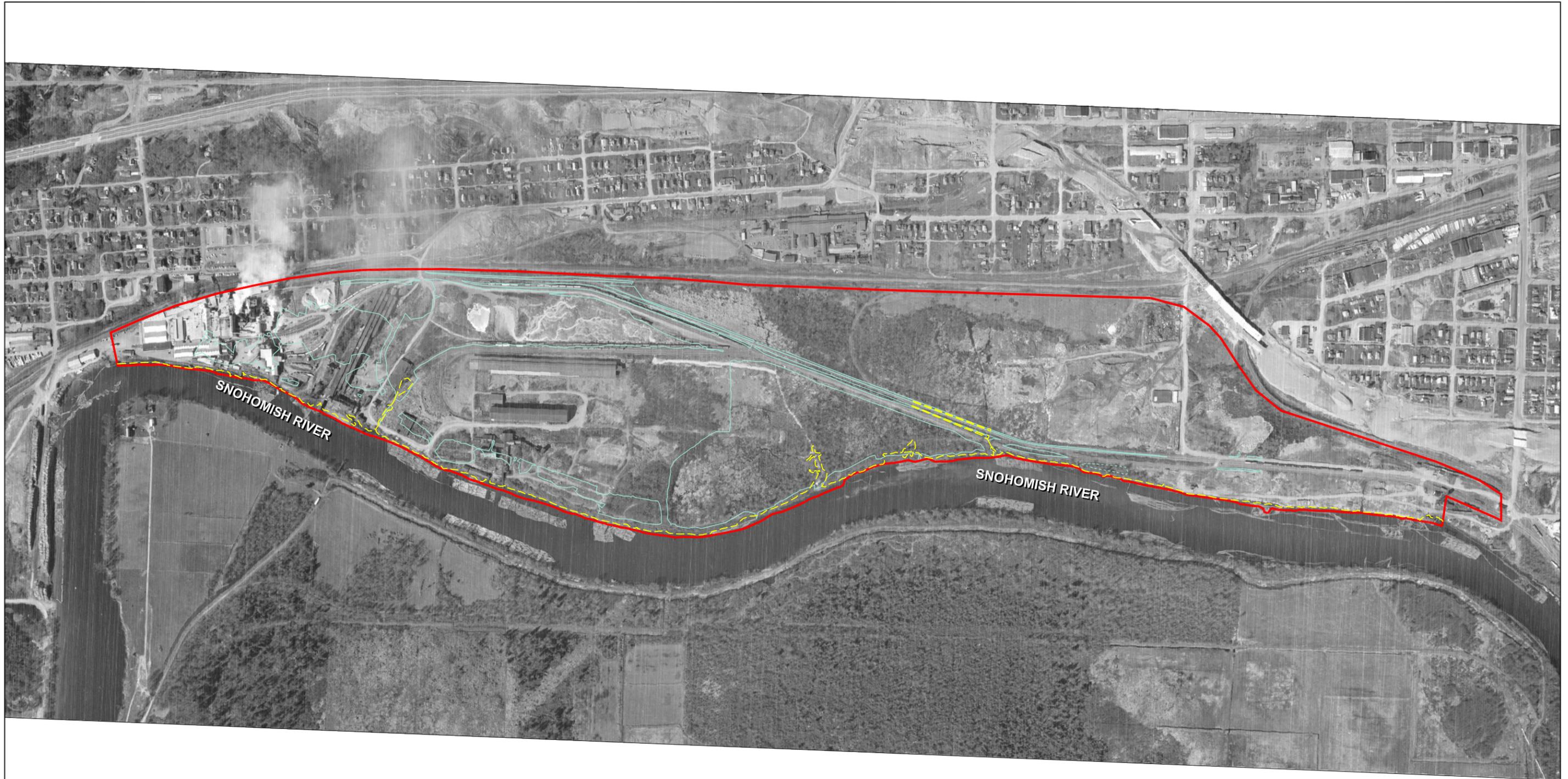
Aerial photos obtained from AERO_METRIC (dated 1947, 1955, 1967, 1976, 1981, 1985, 1993, and 2001) and NAIP Imagery (2006). The OHWM and the Snohomish River was delineated from the sketch provided by a GeoEngineers field staff, aerial imagery and GPS data collected with a Trimble GeoXT unit by GeoEngineers field crew on December 12 and 22, 2006.

Legend

- Site Boundary
- Wetlands
- Snohomish River OHWM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 1955	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-4



Notes:

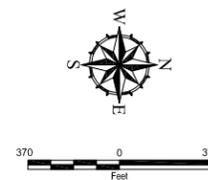
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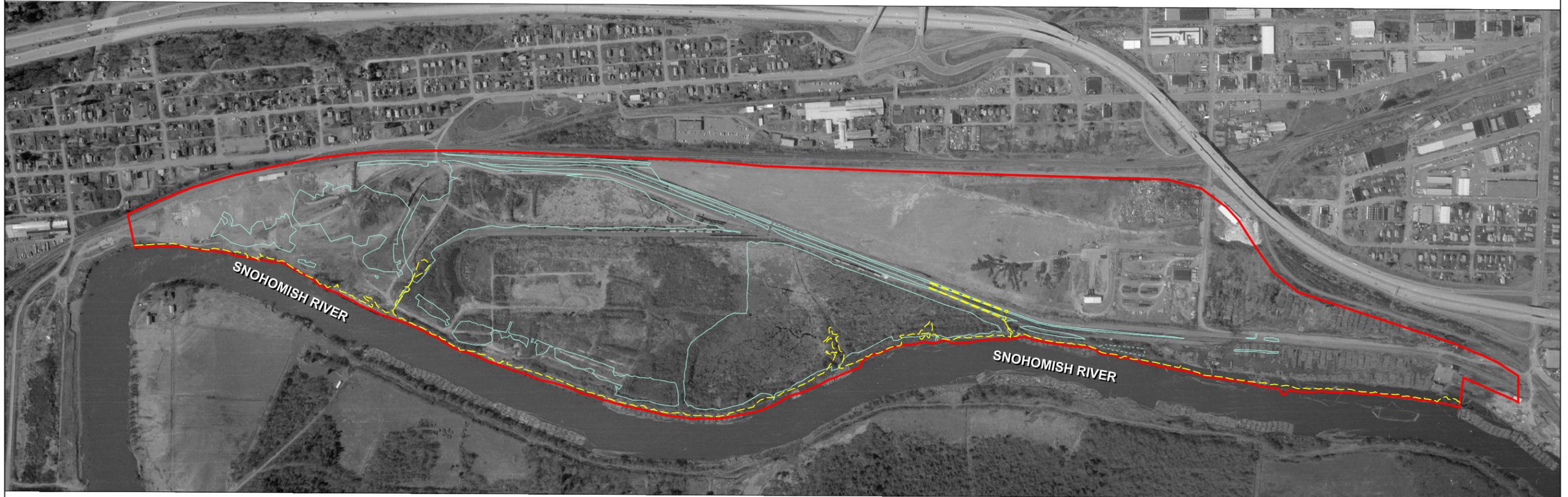
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Legend

- Site Boundary
- Wetlands
- Snohomish River OHWM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 1967	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-5



Notes:

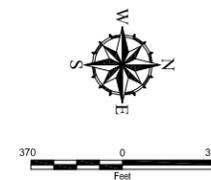
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Data Sources:

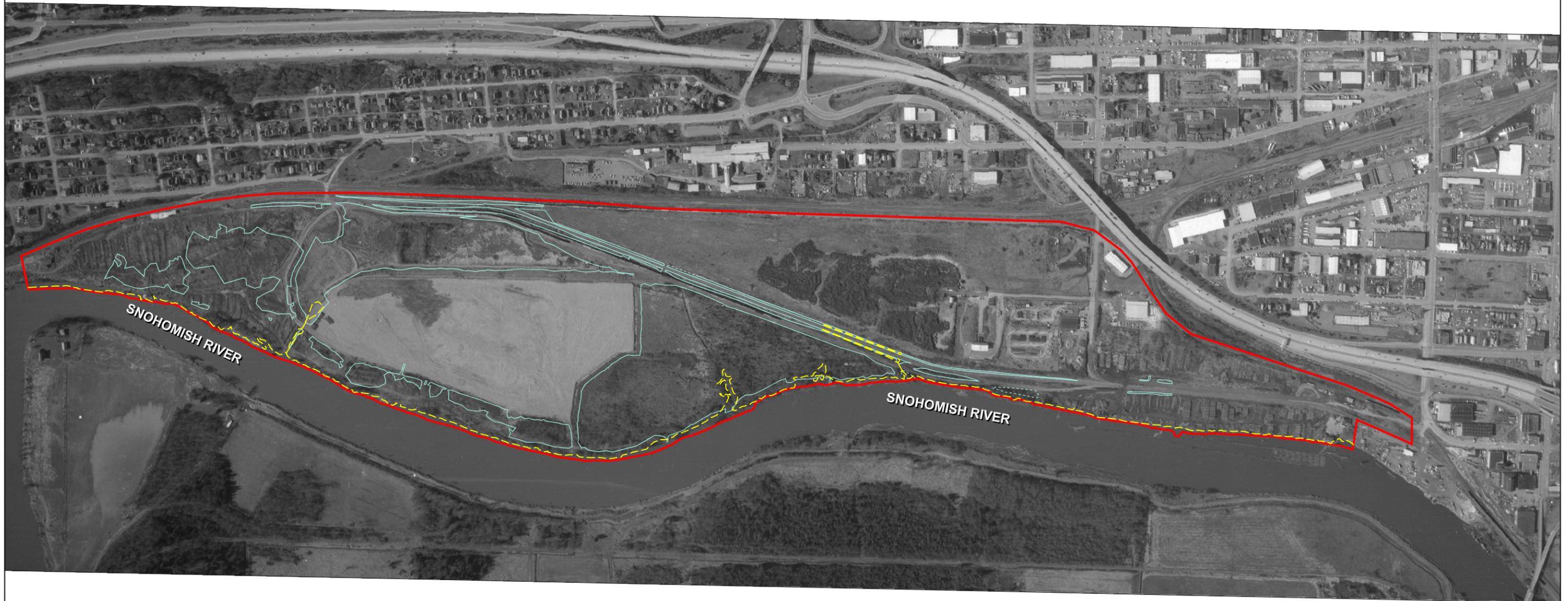
Aerial photos obtained from AERO_METRIC (dated 1947, 1955, 1967, 1976, 1981, 1985, 1993, and 2001) and NAIP Imagery (2006). The OTHM and the Snohomish River was delineated from the sketch provided by a GeoEngineers field staff, aerial imagery and GPS data collected with a Trimble GeoXT unit by GeoEngineers field crew on December 12 and 22, 2006.

Legend

- Site Boundary
- Wetlands
- Snohomish River OTHM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 1976	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-6



Notes:

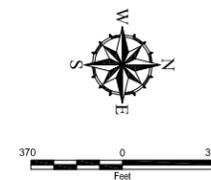
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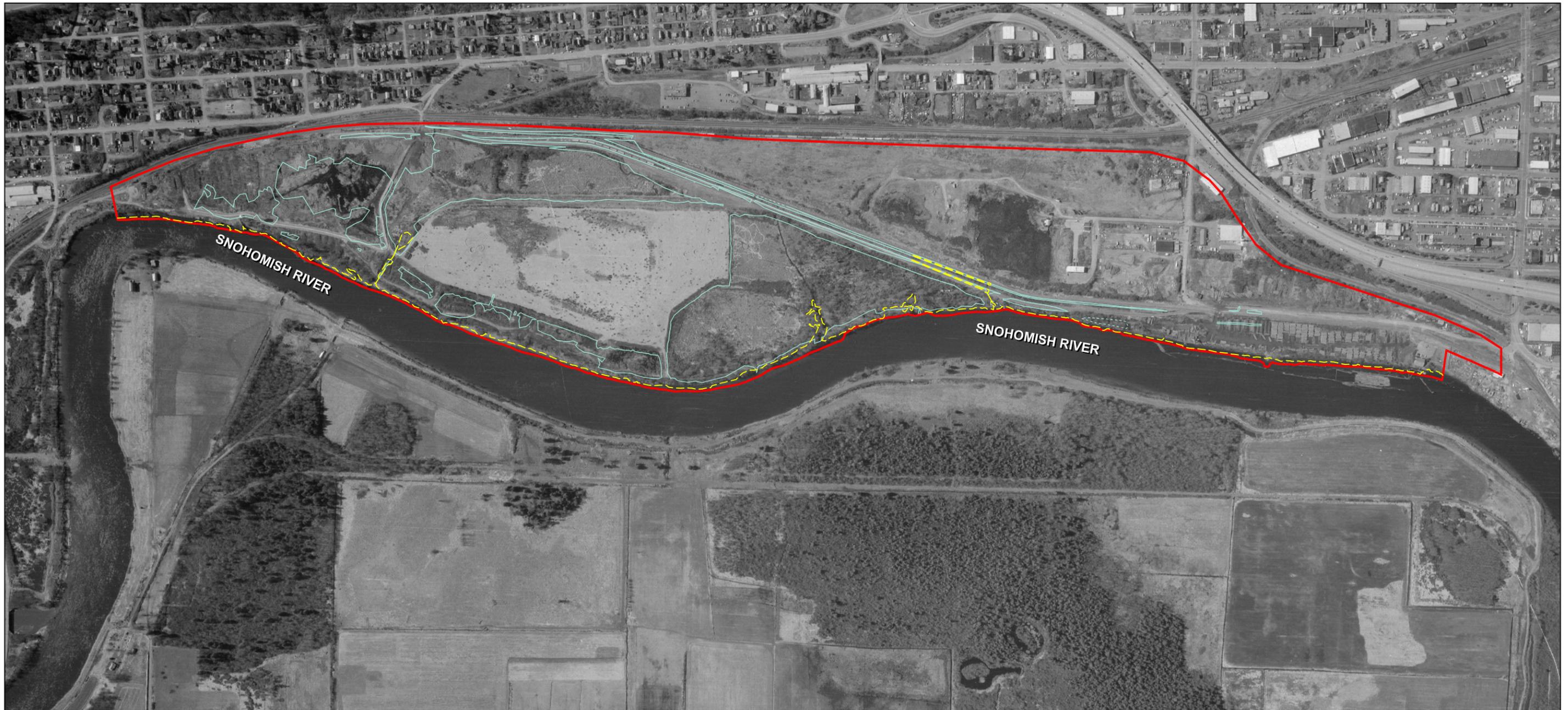
Aerial photos obtained from AERO_METRIC (dated 1947, 1955, 1967, 1976, 1981, 1985, 1993, and 2001) and NAIP Imagery (2006). The OHWM and the Snohomish River was delineated from the sketch provided by a GeoEngineers field staff, aerial imagery and GPS data collected with a Trimble GeoXT unit by GeoEngineers field crew on December 12 and 22, 2006.

Legend

- Site Boundary
- Wetlands
- Snohomish River OHWM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 1981	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-7



Notes:

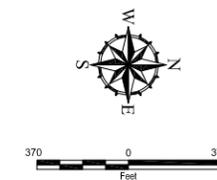
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Data Sources:

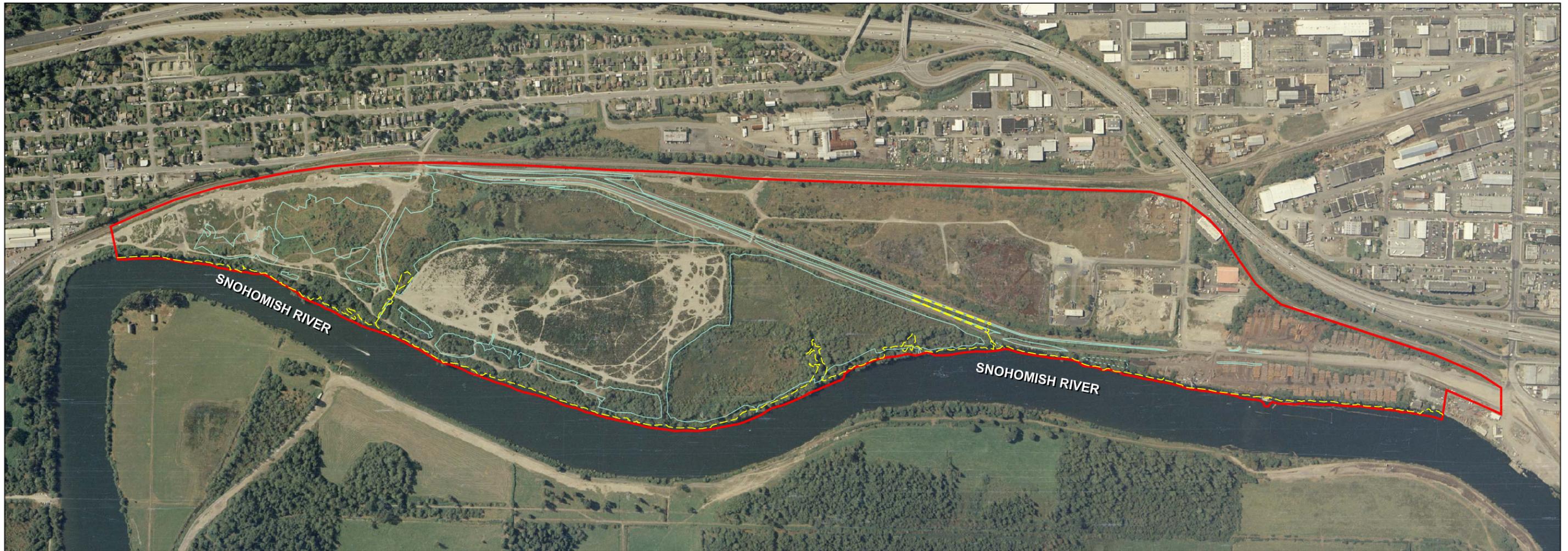
Aerial photos obtained from AERO_METRIC (dated 1947, 1955, 1967, 1976, 1981, 1985, 1993, and 2001) and NAIP Imagery (2006). The OCHM and the Snohomish River was delineated from the sketch provided by a GeoEngineers field staff, aerial imagery and GPS data collected with a Trimble GeoXT unit by GeoEngineers field crew on December 12 and 22, 2006.

Legend

- Site Boundary
- Wetlands
- Snohomish River OCHM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 1985	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-8



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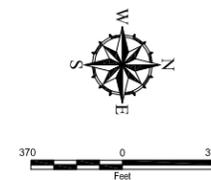
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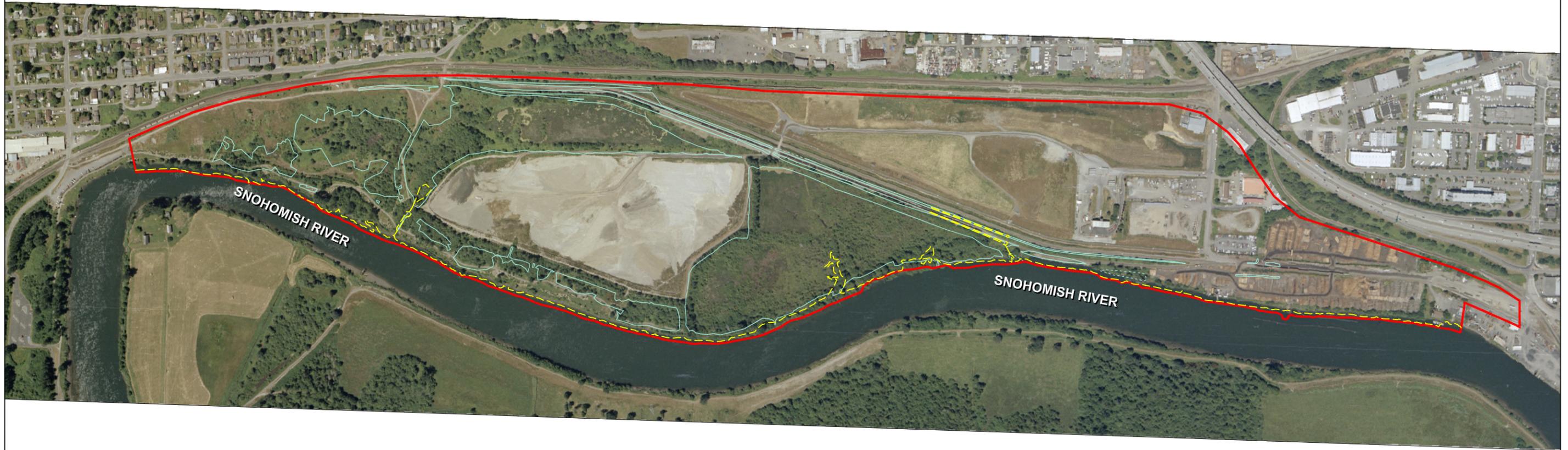
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Legend

- Site Boundary
- Wetlands
- Snohomish River OHWM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 1993	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-9



Notes:

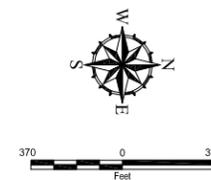
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Legend

- Site Boundary
- Wetlands
- Snohomish River OHWM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 2001	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-10



Notes:

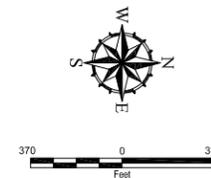
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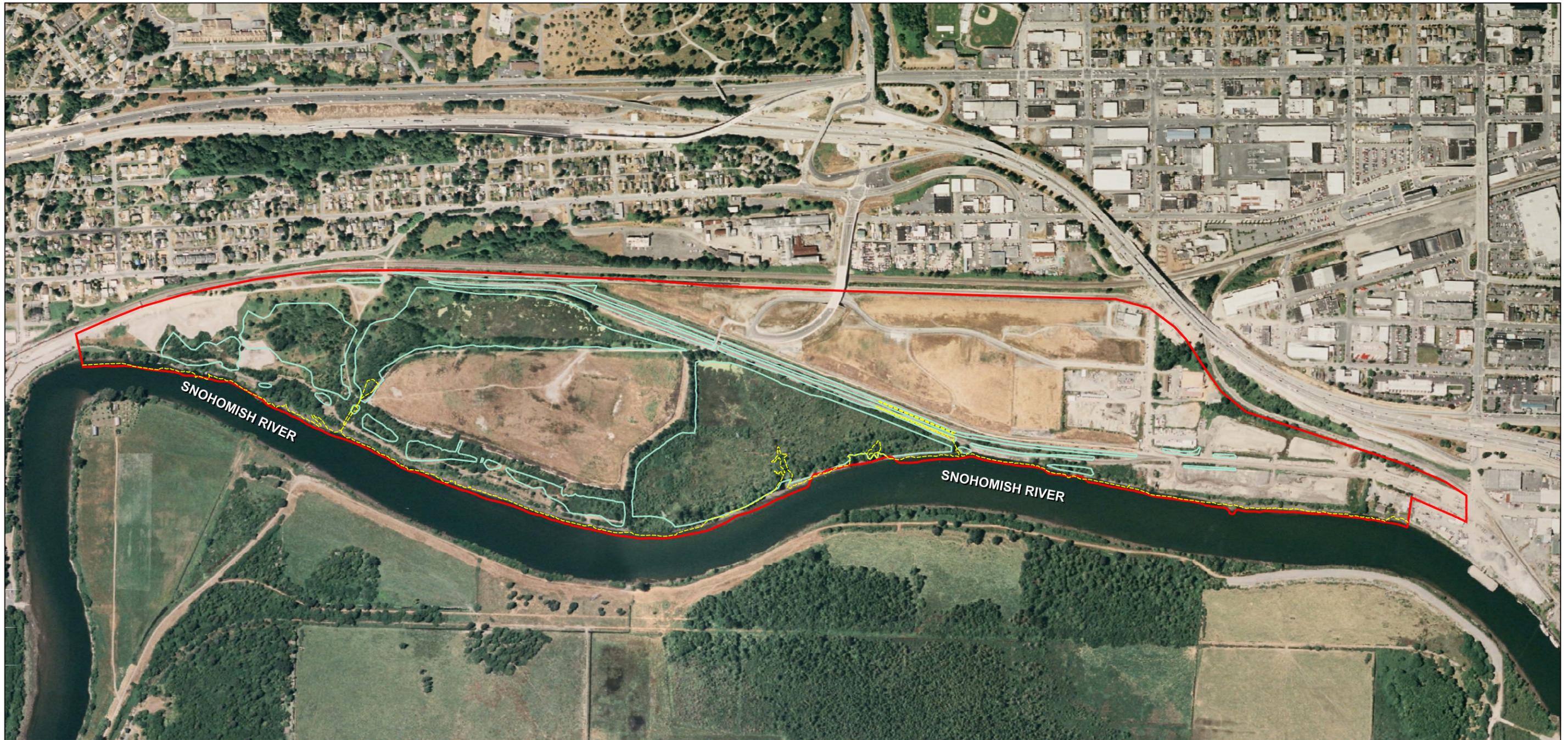
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Legend

- Site Boundary
- Wetlands
- Snohomish River OTHM
- Estimated Boundary not Surveyed



Historic Aerial Photo - 2006	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-11



Notes:

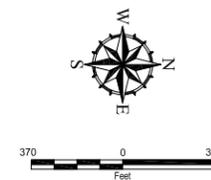
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Legend

- Site Boundary
- Wetlands
- Snohomish River OHWM



Historical Aerial Photo - 2006	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-12

Table 2.1-1. Landfill/Tire Fire Site Historical Land Use

Date	Owner	Use
1917 to 1975	City of Everett/railroads	Landfill, railroad operations
1977 to 1983	City of Everett/railroads	Tire recycling, Public Works yard, Snohomish County solid waste transfer station
1992 to present	City of Everett	City of Everett Animal Shelter, Public Works yard, Snohomish County solid waste transfer station
2003 to present	City of Everett	City of Everett Animal Shelter, Public Works yard

2.1.2 Simpson Pad

This area is generally south and west of the area described below as the Simpson Category 1 Wetlands and Riparian Corridor, and north of the area described below as the South Simpson Site. This area is identified in the Everett Comprehensive Plan and related documents as the “Developable Portion of the Simpson Site.”

Alterations of the site began as far back as 1891, with the addition of the paper pulp mill, and again in the 1930s (see Figures 2.1-4 through 2.1-12, Historical Aerial Photographs), with the addition of earthen dikes (Haring, 2002). The paper mill stayed in operation through 1992, when it was closed and the associated buildings were removed. Table 2.1-2 summarizes the historical land use.

Table 2.1-2. Simpson Site Historical Land Use

Date	Owner	Use
1891 to 1972	Everett Pulp and Paper Company	Paper and pulp production
Between 1891 and 1972	O.W. Plywood Company	Plywood production
Between 1891 and 1972	Walton Lumber Company	Sawmill
1951 to 1992	Lee Paper Mill/ Simpson Paper Mill	Paper mill
1992 to present	City of Everett	Public ground

The Simpson Pad was constructed for development purposes with fill placed during previous industrial activities, hydraulic fill originating from the Snohomish River, and recent fill placed by the City. The following list provides an approximate amount and history of placement of fill materials:

- 1980 - 700,000 cubic yards (cy) of fill was placed on approximately 45 acres.
- 1989 - 450,000 cy of fill was placed on 70.2 acres including 2.1 acres of wetland.
- 1991 - 15,000 cy of fill was placed across 6.5 acres.
- 2000 - 300,000 cy of fill was placed over the same 45 acres filled in 1980.
- 2000 - 25,000 cy of fill was placed on the site.
- 2006 - 7,500 cy of fill was placed to construct a roadway.

2.1.3 Simpson Category 1 Wetlands and Riparian Corridor

This area is located south of the Landfill/Tire Fire site, with the BNSF mainline on the east, and between the Snohomish River and the upland area known as the Simpson Pad or the Developable Portion of

Simpson Site (see Figures 2.1-4 through 2.1-12, Historical Aerial Photographs). This area consists or will consist entirely of aquatic or riparian habitat and public access.

There are approximately 50 acres of wetland located around the Simpson site which are commonly found adjacent to the Snohomish River and the East Ditch (Shoreline Management Application #78-008, #86-001, #95-005, #05-014 and #05-015). Most of the wetlands are concentrated on the southern portion of the project site surrounding the majority of the Simpson Pad and including lower Bigelow Creek. Approximately 30 acres of wetlands are found on the portion known as the Simpson Category 1 Wetlands and Riparian Corridor (see Figures 2.1-13 through 2.1-13B, Wetland Delineation and Buffer Maps).

Approximately 25 acres located on the South Simpson property were previously filled during the past 30-plus years since the Simpson site has been inactive as an active industrial site. In the past roughly 10 acres of wetlands were filled in connection with the railroad track construction on the western portion of the project site, and approximately 15 acres of wetlands located on the southern portion of the site were also filled (U.S. Army Corp of Engineers FY 1979 Maintenance Dredging, Upper Snohomish River Navigation Channel and Settling Basin at Everett, WA., Ref. NPSOP-NF-32, 1979).

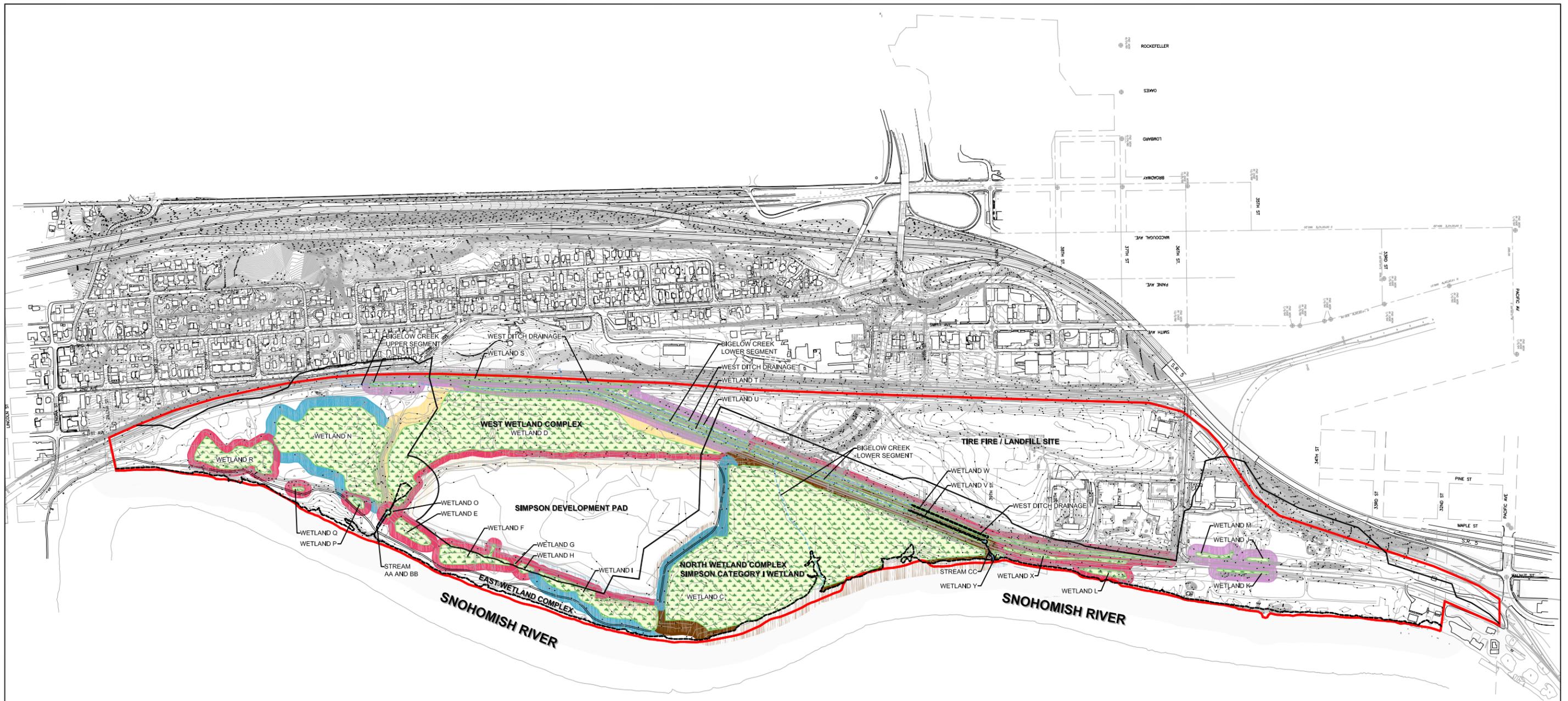
2.1.4 South Simpson Site

This area lies between the BNSF mainline on the west, the Snohomish River on the east, the Washington Department of Transportation (WSDOT) water treatment property south and west of the Simpson Pad. This area consists or will consist entirely of aquatic or riparian habitat and public access (see Figures 2.1-4 through 2.1-12, Historical Aerial Photographs).

2.1.5 Eclipse Mill Site, Drywall Site, Port of Everett, Stuchell and Newland

This area lies north of the easterly extension of 36th Street, east of the BNSF 'C'-line track and right-of-way, west of the Snohomish River and south of Pacific Avenue. The Port of Everett, Stuchell Enterprises and Newland Construction also own properties in this area that are included in the proposed redevelopment.

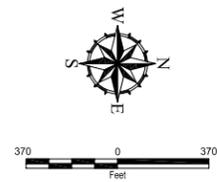
In the Eclipse Mill portion of the project site, H.O. Sieffert and Company's Shingle Mill was between Pacific Avenue and 32nd Street, east of the Northern Pacific and Monte Cristo line and north of the Eclipse Mill (Northwest Archeological Associates, Inc. *Cultural Resources Assessment for the Everett Riverfront Master plan and Redevelopment Project, Snohomish County, Washington* dated April 2007). Just south of 32nd Street, Gould, Keen and Wright produced shingles beginning in 1899 until W.I. Carpenter and H.W. Stuchell purchased the shingle mill in 1903(see Figures 2.1-4 through 2.1-12, Historical Aerial Photographs) and built the Eclipse Lumber Company Saw, Shingle and Planing Mill (Taylor, 2002; Baker, 1967:7, 19). Stuchell's sons continued the business and renamed it the Eclipse Lumber Company, Inc. in 1948. A fire destroyed the mill in 1962, leaving only the water tank and office, although the company continued buying and selling timber until 2004 when property still retained by Eclipse was sold to the City of Everett (other than one acre retained by Eclipse Properties LLC and a parcel at the extreme north end of the site was sold to Newland Construction in 1983). The Milwaukee Chicago and St. Paul railroad owned land north of 36th Street and west of the Eclipse Mill properties which were sold to a drywall operation (located adjacent to 36th Street and the railroad mainline) and the Port which developed buildings used for a sheltered workshop.



Legend

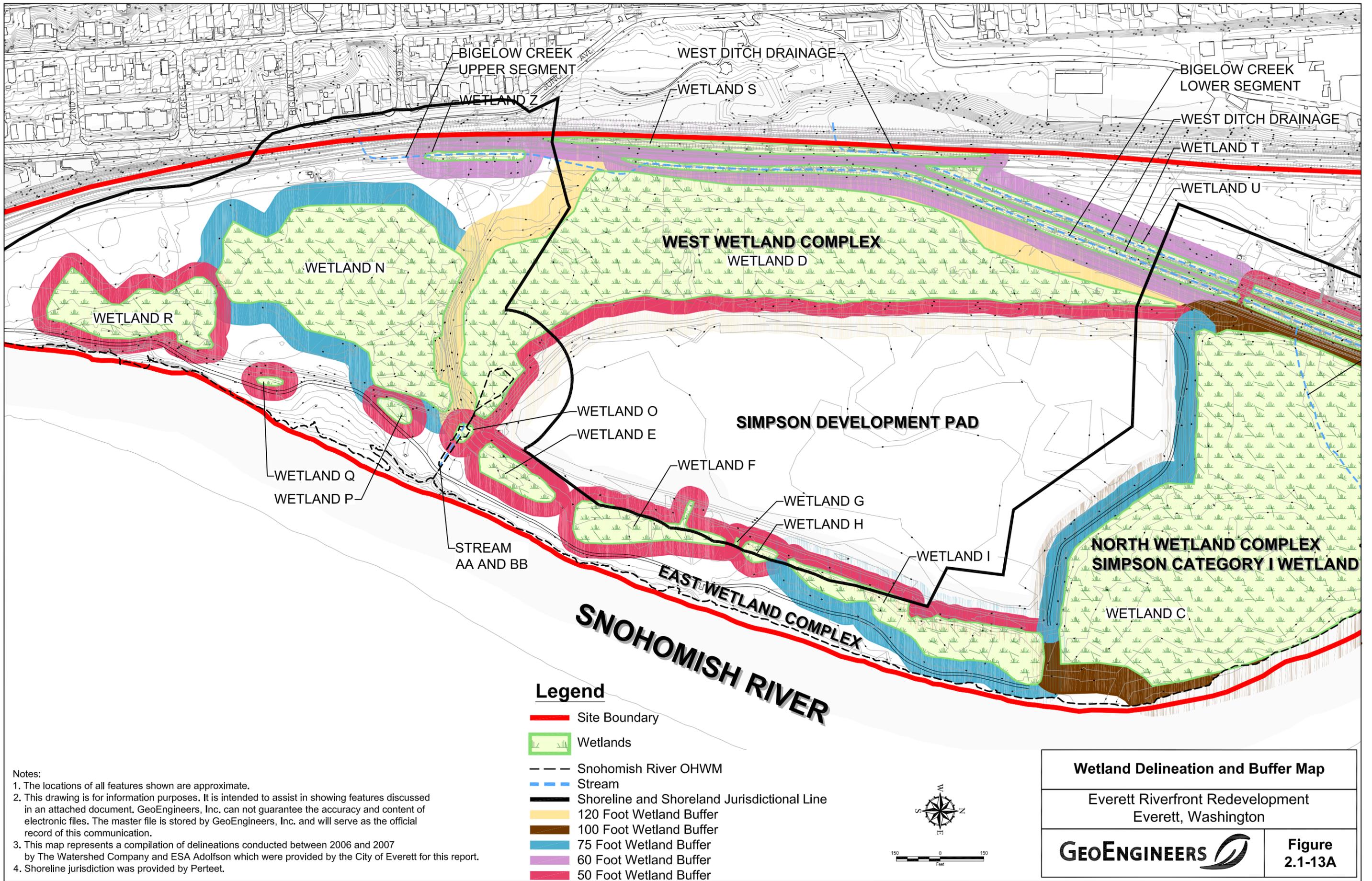
- Site Boundary
- Wetlands
- Snohomish River OHWM
- Stream
- Shoreline and Shoreland Jurisdictional Line
- 120 Foot Wetland Buffer
- 100 Foot Wetland Buffer
- 75 Foot Wetland Buffer
- 60 Foot Wetland Buffer
- 50 Foot Wetland Buffer

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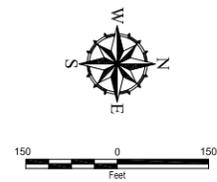


Wetland Delineation and Buffer Map	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-13

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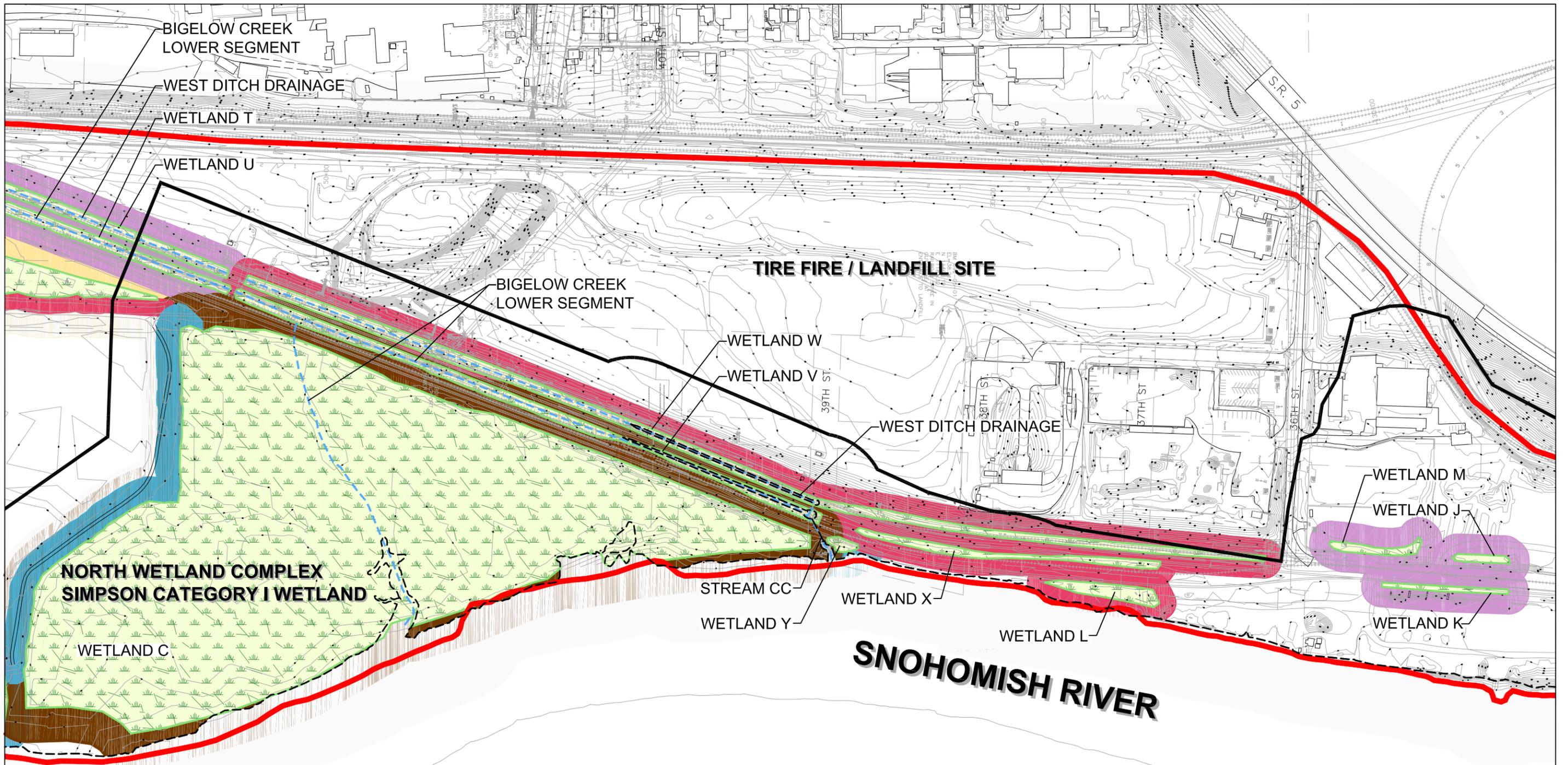


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Wetland Delineation and Buffer Map	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-13A

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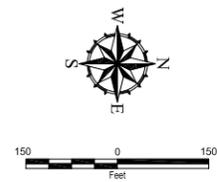


Legend

- Site Boundary
- Wetlands
- Snohomish River OHWM
- Stream
- Shoreline and Shoreland Jurisdictional Line
- 120 Foot Wetland Buffer
- 100 Foot Wetland Buffer
- 75 Foot Wetland Buffer
- 60 Foot Wetland Buffer
- 50 Foot Wetland Buffer

Notes:

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- This map represents a compilation of delineations conducted between 2006 and 2007 by The Watershed Company and ESA Adolphson which were provided by the City of Everett for this report.
- Shoreline jurisdiction was provided by Perteeet.



Wetland Delineation and Buffer Map	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.1-13B

Table 2.1-3. Eclipse Mill Site, Drywall Site, Port of Everett, Stuchell and Newland Land Use

Date	Owner	Use (Owner)
Circa 1900	H.O. Sieffert and Company and Milwaukie RR	Shingle Mill
1903 to 1962	W.I. Carpenter and H.W. Stuchell Milwaukee RR	Eclipse Lumber Company Saw, Shingle and Planing Mill
1962 to 2004	Eclipse Mill (Stuchell), Newland Construction (1983), Port of Everett, GTS Drywall	Miscellaneous log handling and chipping operations and construction office (Newland), Sheltered workshop (Port), drywall distribution
2004 to present	City of Everett/Eclipse Properties LLC/Newland/Port of Everett	Stockpiling construction dirt (City), Diversified Industries (Port), Construction offices (Newland), vacant (Eclipse Properties LLC, City "Drywall" parcel)

2.2 DESCRIPTION OF THE PROPOSED DEVELOPMENT

The proposed redevelopment will include construction of a mixed-use commercial/residential development, shoreline and habitat restoration, and rehabilitation of a former, mostly industrial site. The proposed Master Plan includes the construction of up to 900,000 square feet of mixed commercial use; 200,000 square feet of hotel space; and up to 1,400 residential units (multi- and single-family). A schedule of the timing of the proposed development is shown in Table 2.2-1, Everett Riverfront Milestone Schedule, below. The ultimate mix of uses constructed will be determined by market demand and the land use capacity of the site (type, location, and size of uses and structures, and infrastructure capacity). The Master Plan may be amended over time in response to market demand for the proposed uses. The preferred alternative is shown in Figures 2.3-1 through 2.3-1B, Preferred Alternative Layout.

Table 2.2-1. Everett Riverfront Milestone Schedule

Parcel	Action	Date
	City reviews, revises, and issues Final EIS	February 2008
Landfill/Tire Fire	Building permits	August 2008
Landfill/Tire Fire	Zoning approvals complete	May 2008
Landfill/Tire Fire	Shoreline permits complete	May 2008
Simpson Pad	Grading permit	August 2009
Simpson Pad	Receive building permit	March 2010
Eclipse Mill/Drywall	Road permit	March 2009
Landfill/Tire Fire	Grading permit	September 2009
Landfill/Tire Fire	Receive building permit	February 2010
	Begin roundabout and bridge to Simpson Site	March 2008
Landfill/Tire Fire	Landfill grading, fill, and surcharge	February 2008
Landfill/Tire Fire	Utility installation	March 2009
Landfill/Tire Fire	Landfill gas system installation	May 2009
Landfill/Tire Fire	Building construction	March 2009
Landfill/Tire Fire	Site work	May 2009
Landfill/Tire Fire	Occupancy	September 2010

Parcel	Action	Date
Simpson Pad	Begin grading	October 2009
Simpson Pad	Begin construction	March 2010
Simpson Pad	Occupancy	May 2011
Eclipse Mill/Drywall	Begin grading	May 2009
Eclipse Mill/Drywall	Road	December 2009
Eclipse Mill/Drywall	Begin construction	March 2010
Eclipse Mill/Drywall	Occupancy	May 2011
	Construct dock below gathering place	May 2010
	City wetland enhancement and trail extension	July 2010
	City Park at 36 th Street	July 2010

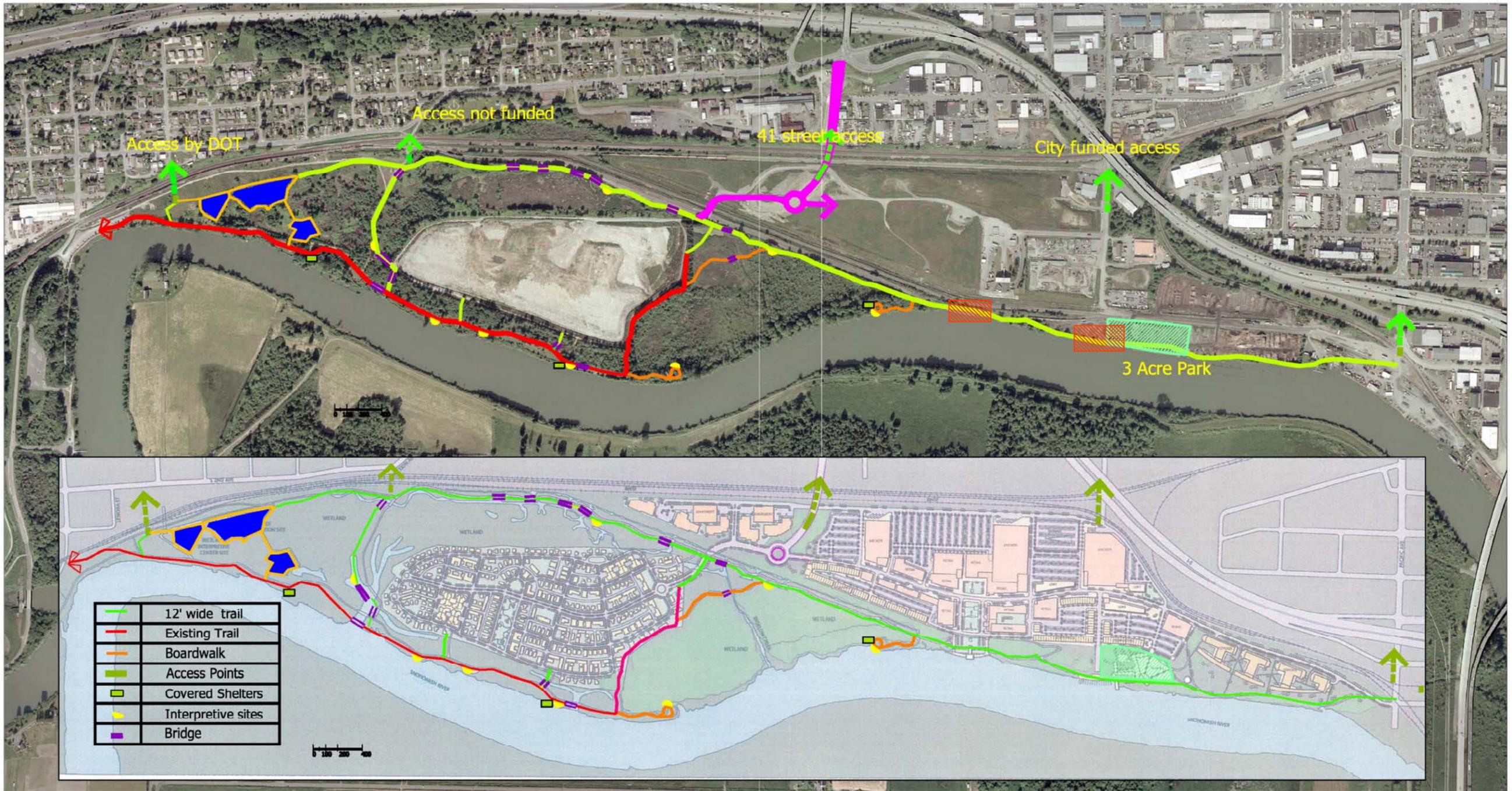
Construction associated with grading and earthmoving activities are estimated to begin spring 2008 for areas outside of Shoreline jurisdiction after the appropriate application process and receipt of applicable permits. It is estimated that final grading and earthmoving activities will be completed by autumn of 2009, with building construction estimated to begin on various portions of the site by summer of 2010.

The proposal includes a variety of public amenities. Amenities to be provided by OM are addressed in this DEIS, and include (see Figure 2.2-2, Existing and Proposed Trails and Public Access and Improvements):

- Wetland and buffer mitigation and enhancements;
- A 1 1/2-acre “public gathering area” near the center of the landfill/tire fire site;
- A boat house, dock and ancillary facilities connected to the public gathering space;
- Park spaces on the Simpson development pad;
- A relocated trail along the northern portion of the Simpson Pad linking the existing trail along the River with the eastern extent of the 41st Street Bridge to the Simpson Pad and the to-be-abandoned BNSF ROW;
- A temporary gravel trail along the shoreline and on the abandoned railroad ballast from 36th Street to the south end of the landfill site;
- A trail along the River within Eclipse Mill site;
- Bridges across the proposed wetland mitigation between the Simpson Pad and the Snohomish River to maintain connections in the existing trail.

Public amenities to be provided by the City, with specific improvements to be addressed in a supplemental SEPA review include

- A 3-acre park, which may include open space for unscheduled use, viewing and gathering areas, trails restroom facilities, picnic areas or shelters, interpretive or arts amenities, waterfront access facilities, and various park amenities to support passive enjoyment of the riverfront setting.
- Permanent trails where OM improvements include temporary gravel trail improvements.



Everett Riverfront Development Conceptual Public Amenities

The Riverfront development will add 2.3 miles of trails to the existing 1.2 miles of trails at Lowell Riverfront trail.

The site will include 78 acres of natural areas and wetlands to explore

3 acres of neighborhood park with a playground, restroom, grass area large enough to play soccer and support community events.

 Approximate Location of Proposed Docks

December 21, 2006

Notes:

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Reference: Drawing obtained from Oliver McMillan.

Plans for many of the public amenities including trail extensions, park areas, wetland restoration and enhancements that are being undertaken by the City as part of the Project are still at a conceptual level and are described within this environmental review generally unless otherwise noted. The City of Everett is committed to completing the desired improvements to public amenities. Additional environmental review to permit these amenities will be completed when design details are developed more fully over time. (It is currently anticipated that additional SEPA analysis and permit applications for a portion of the work will be completed in April 2008.)

Existing & Conceptual Trails and Public Access Improvements		Figure 2.2-1
Everett Riverfront Redevelopment Project Everett, Washington		

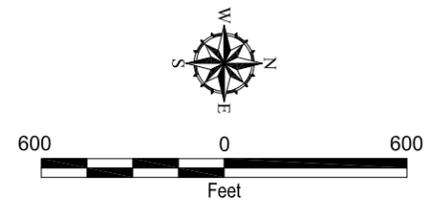


RESIDENTIAL USES SEE 2.3-1A FOR CONCEPT DETAIL

MIXED USES SEE 2.3-1A AND B FOR CONCEPT DETAIL

MULTI-FAMILY RESIDENTIAL AND MIXED USES SEE 2.3-1B FOR CONCEPT DETAIL

LEGEND	
	WESTWOOD TOWNHOMES
	CLUSTER TOWNHOMES:
	ROWHOME - LANDINGS:
	COURTYARD HOMES - TAMBARK:
	TRIPLEXES:
	PASEO HOMES - GREENS:
	DUPLEX LOTS:
	ZIPPER LOTS:



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Reference: Image provided by Gensler.

Preferred Alternative	
Everett Riverfront Redevelopment Everett, Washington	
	Figure 2.3-1

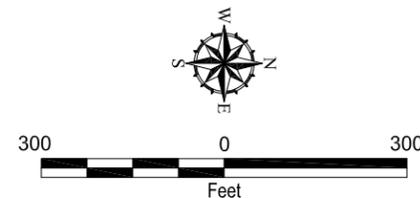


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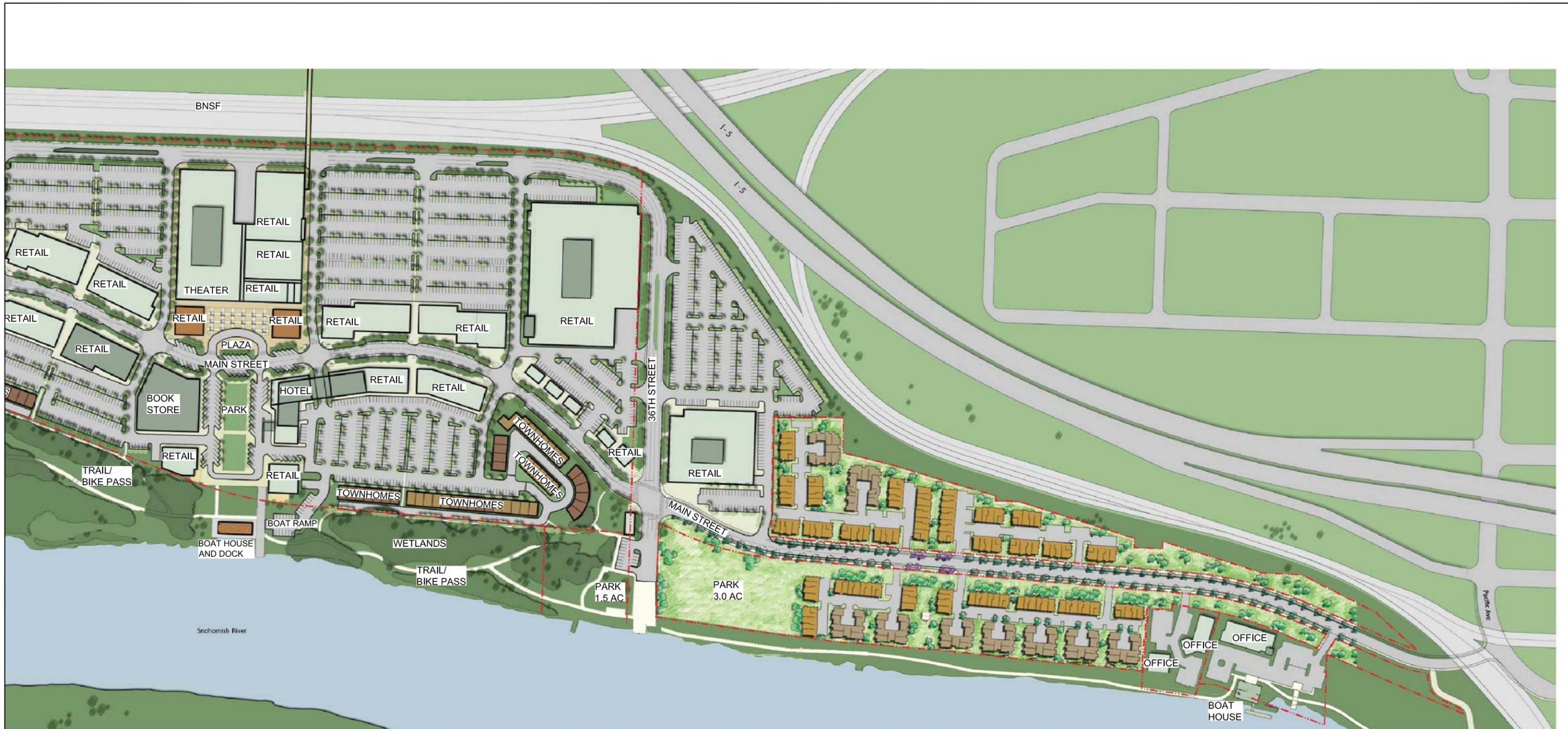
Reference: Image provided by Gensler.

LEGEND	
	WESTWOOD TOWNHOMES
	CLUSTER TOWNHOMES
	ROWHOME - LANDINGS
	COURTYARD HOMES - TAMEARK
	TRIPLEXES
	PASEO HOMES - GREENS
	DUPLEX LOTS
	ZIPPER LOTS



Preferred Alternative - South	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.3-1A

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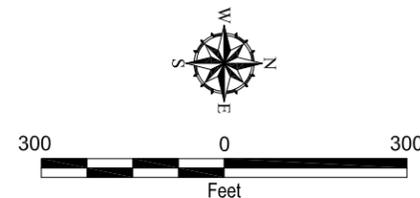
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Reference: Image provided by Gensler.

LEGEND

RESIDENTIAL

- CLUSTER TOWNHOMES:**
(1,183 S.F. - 1,548 S.F.)
- ROWHOMES:**
(1,236 S.F. - 2,014 S.F.)



Preferred Alternative - North	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS	Figure 2.3-1B

- Shoreline edge treatments along the Snohomish River between the 3-acre park and the parking area for the Lowell Riverfront Trail.
- Stream, wetland and buffer restoration actions with associated trails and facilities.

The City is separately evaluating detailed designs and potential impacts of these facilities. The *Snohomish Riverfront Properties at Bigelow Creek: Conceptual Enhancement Program*, Watershed Co, 2005, hereinafter referred to as the Watershed Conceptual Program, and Exhibit I to the PDA are being used as a starting point for designing the improvements. The Watershed Conceptual Program includes a variety of potential enhancement and restoration actions, such as restoring floodplain and tidal connections, reconnection of wetland complexes, relocating and reconstructing the Bigelow Creek channel, improving edge habitat, and restoring and enhancing the vegetation community. The program also includes trails, wetland viewing areas, and interpretive facilities, including a potential interpretive / cultural center. The separate detailed analysis currently being completed is evaluating the feasibility of these conceptual designs, and may result in alternative proposed improvements / restoration / enhancement action. The PDA agreement requires the City to substantially complete the improvements within 36 months of obtaining the non-City governmental permits and approvals for the work. These public amenity / restoration / enhancement activities are not considered mitigation for the current project. The City's detailed designs, supplemental SEPA review, and shoreline permit applications for the public amenities are expected to be completed for public review in spring 2008.

The proposal includes the rezone of properties at the north end of the site where development proposals have not yet been developed (Newland and Stuchell properties north of the city park). The public access trail will be continued north through those properties to Pacific Avenue. Temporary trail improvements along the public road may be required prior to redevelopment of these properties.

The proposed development will be designed and constructed using sustainable building and development practices such as those found in the U.S. Green Building Council's Leadership in Energy and Environmental Design (LEED) system. The Council's Core Purpose is to transform the way buildings and communities are designed, built and operated, enabling an environmentally and socially responsible, healthy and prosperous environment that improves the quality of life (US Green Building Council, 2006, LEED for New Construction Version 2.2 Reference Guide).

2.3 PROJECT ALTERNATIVES

2.3.1 Alternative 1 (Preferred Alternative)

The construction of up to 900,000 square feet of mixed commercial uses; up to 200,000 square feet of hotel space mostly on the Landfill/Tire Fire site; and up to 1,400 residential units and associated public amenities throughout the project site, see Figure 2.3-1.

2.3.2 Alternative 2

The Simpson Pad would be developed with approximately 700,000 square feet of office space. Development of the Landfill/Tire Fire site would receive approximately 700,000 square feet of commercial (retail) space with development of the Eclipse Mill site receiving approximately 200,000 square feet of commercial (retail) and associated public amenities (see Figures 2.3-2 through 2.3-2B and 2.3-3 through 2.3-3B).

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38-FOOT TALL OFFICE BUILDINGS

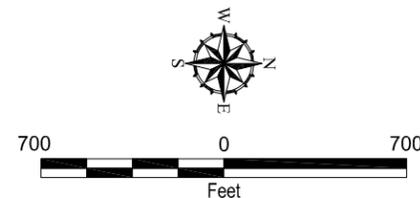
RETAIL BUILDINGS

OFFICE BUILDINGS

Notes:

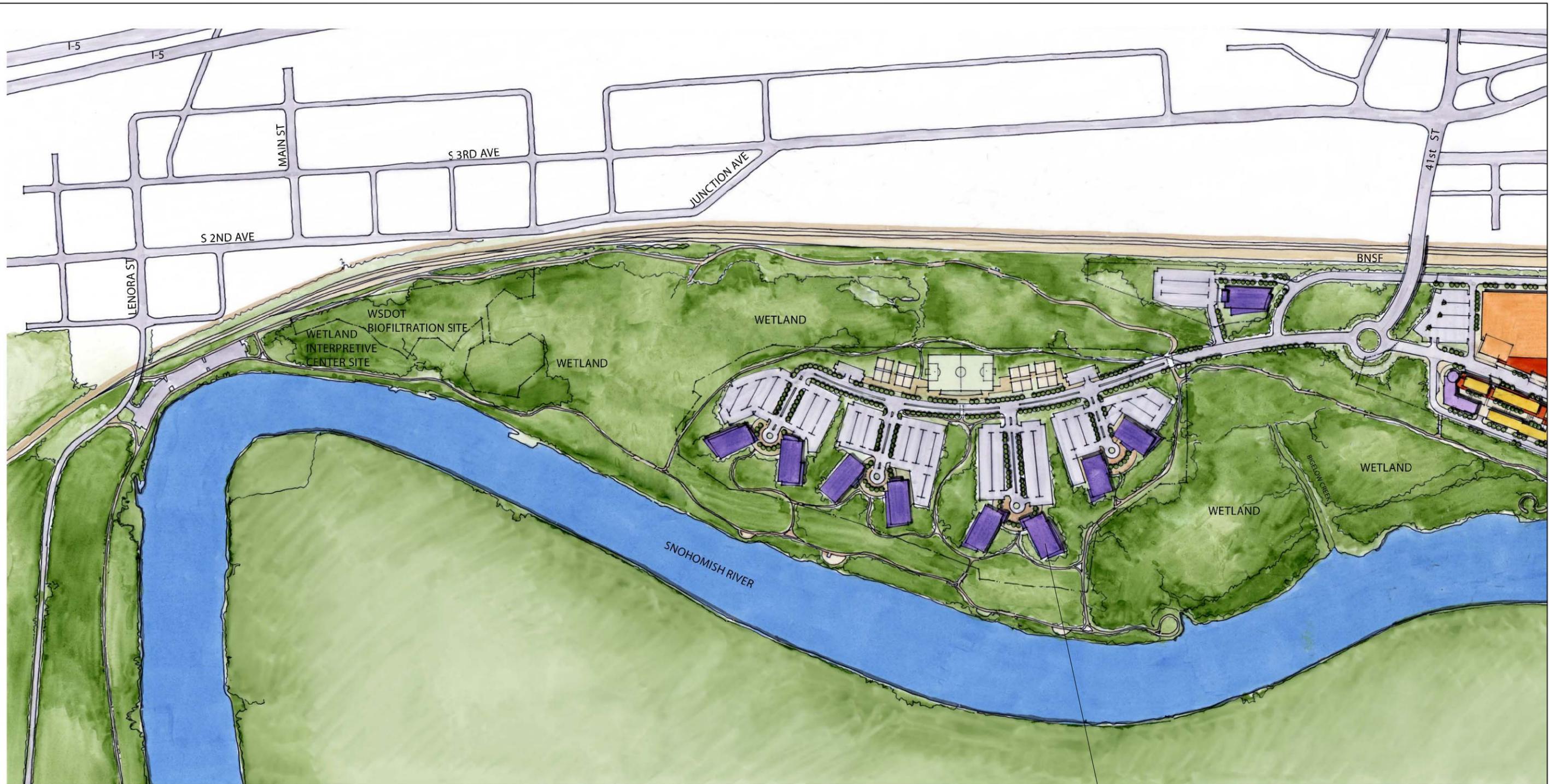
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Reference: Image provided by Gensler.



Alternative 2 Proposed Site Layout	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS 	Figure 2.3-2

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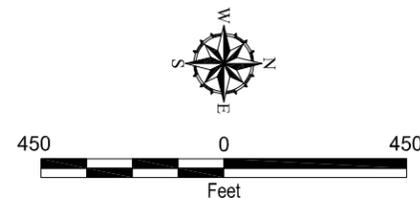


38-FOOT TALL OFFICE BUILDINGS

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Reference: Image provided by Gensler.



Alternative 2 Proposed Site Layout - South

Everett Riverfront Redevelopment
Everett, Washington



Figure
2.3-2A

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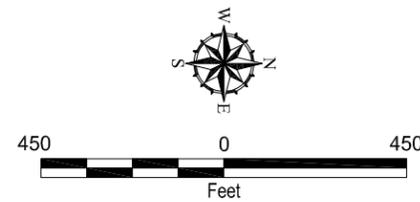
MIXED USE
COMERCIAL / RETAIL

OFFICES

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Reference: Image provided by Gensler.



Alternative 2 Proposed Site Layout - North

Everett Riverfront Redevelopment
Everett, Washington



**Figure
2.3-2B**

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77-FOOT OFFICE BUILDINGS
 PARKING GARAGES

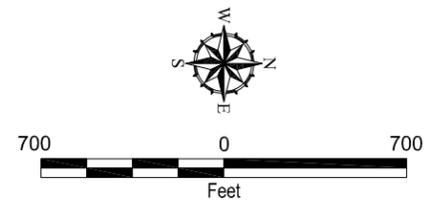
RETAIL

OFFICE

Notes:

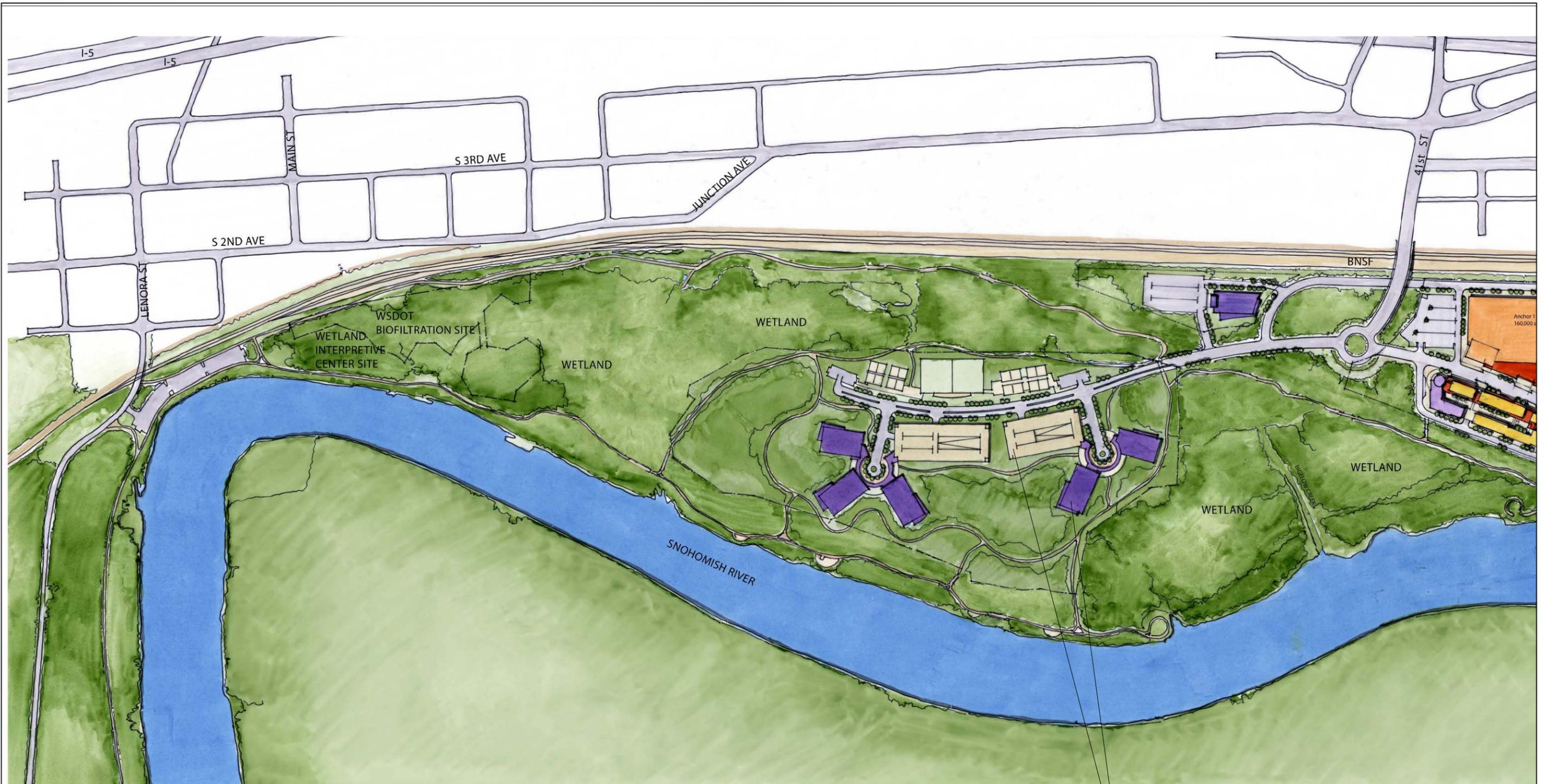
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Reference: Image provided by Gensler.



Alternative 2 (Option 2) Proposed Site Layout	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS 	Figure 2.3-3

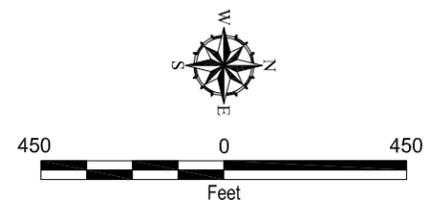
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77-FOOT OFFICE BUILDINGS
PARKING GARAGES

- Notes:
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Reference: Image provided by Gensler.



Alternative 2 (Option 2)	
Proposed Site Layout - South	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS 	Figure 2.3-3A

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77-FOOT OFFICE BUILDINGS
PARKING GARAGES

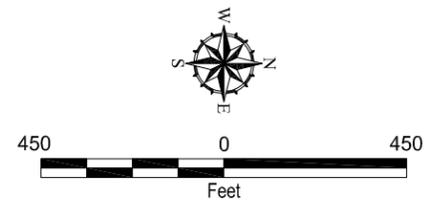
MIXED USE
COMERCIAL / RETAIL

OFFICES

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Reference: Image provided by Gensler.



Alternative 2 (Option 2)	
Proposed Site Layout - North	
Everett Riverfront Redevelopment Everett, Washington	
GEOENGINEERS 	Figure 2.3-3B

2.3.3 Alternative 3 (No-Action Alternative)

Under the no-action alternative, the development of commercial/residential spaces and associated public amenities would likely be postponed because it would depend on a future user or users that are not known at this time. Because future development would have to be consistent with the Comprehensive Plan designation and vision, future development impacts would, at a minimum, be similar to Alternative 2. Under the no-action alternative, the City would still complete the environmental restoration and habitat enhancements that are required by settlement agreements among the City, Tulalip Tribes and environmental groups, but these efforts would likely occur over a much longer period of time and would not likely be integrated with other public amenities to the extent proposed under the action alternatives.

2.4 OTHER ACTIVITIES ON AND ADJACENT TO THE SITE

Other projects planned or implemented adjacent to the project site within the last five years include:

2.4.1 41st Street Overcrossing

Bicycle and pedestrian improvements are being provided as part of the recently completed 41st Street Overcrossing. Improvements include 6-foot-wide sidewalks on both sides of the street and 5-foot-wide bicycle lanes.

2.4.2 Interurban Trail

West of the project site, the Interurban Trail currently terminates at Colby Avenue and 41st Street. A current City of Everett project will improve 41st Street between I-5 and Colby Avenue and will include pedestrian/bicycle improvements. These improvements, in conjunction with the 41st Street Overcrossing improvements, will complete the link between the existing termination of the Interurban Trail and the project site, and will connect with the public access improvements proposed as part of the riverfront redevelopment project. Construction of the 41st Street improvements is projected to commence in 2007, with completion anticipated in 2008.

2.4.3 Main Street Pedestrian Overcrossing

A pedestrian overcrossing is currently under construction as part of the WSDOT Stormwater Facility Project. The project includes a pedestrian overcrossing of the BNSF Railroad tracks, at a location that would be an easterly extension of Main Street, and touches down in the area adjacent to the WSDOT stormwater ponds. The overcrossing connection is intended to be for pedestrian use only and will provide a safe pedestrian connection at Main Street between the Lowell Community and the project site.

2.4.4 38th Street Vicinity Overcrossing

A pedestrian/bicycle overcrossing of BNSF mainline tracks is planned for the area between 36th and 41st Streets. This improvement would provide a link between the project site and the Everett Station Area and ultimately to downtown Everett. This improvement is included in the City's Transportation Improvement Program, and the City is securing funds for the improvement.

2.4.5 Pacific Avenue Connection

Pedestrian/bicycle and public access facilities will be provided as an integral part of the project and will extend the full length of the property. Improvements are proposed to extend to the north end of the Eclipse Mill site. Pedestrian improvements will be part of the roadway that extends through the Eclipse Mill site and will connect with improvements on Pacific Avenue. As an interim connection where the

project site ends at the Newland property, the public access trail would loop from the riverside location back to the roadway through the Eclipse Mill site and tie into pedestrian improvements on the roadway.

2.4.6 Additional Connection to the Lowell Community

The existing agreement between the City and BNSF provides for a pedestrian/bicycle overcrossing between the WSDOT overcrossing at Main Street and the 41st Street Overcrossing. At this time, no site has been selected and no funding is currently available for this improvement. The overcrossing would likely be in the general vicinity of Lowell Park.

2.4.7 I-5 HOV Project

The WSDOT is nearing completion of construction of an extension of the HOV lanes on I-5 between SR 526 and SR 2. The I-5 HOV project also includes general-purpose vehicle auxiliary lanes from 41st Street to SR 2. The I-5 HOV project is scheduled to be open to traffic in 2007 and will require access point revisions to the 41st Street/Broadway interchange to revise the existing northbound left-hand general purpose exit to an HOV-only access and replace the general purpose exit with a right-hand exit. A stormwater treatment system to address much of this project area is being developed by WSDOT in the southeast portion of the riverfront redevelopment project site.